



CONSTRUCTION MANAGEMENT SERVICES FOR PROJECT NO. 675 - SEPULVEDA BOULEVARD WIDENING PROJECT

CITY OF CARSON | REQUEST FOR PROPOSAL NO. 21-040

SUBMITTED: December 2, 2021



SUBMITTED BY

EXP U.S. Services, Inc.

451 East Vanderbilt Way | San Bernardino, CA 92408, USA

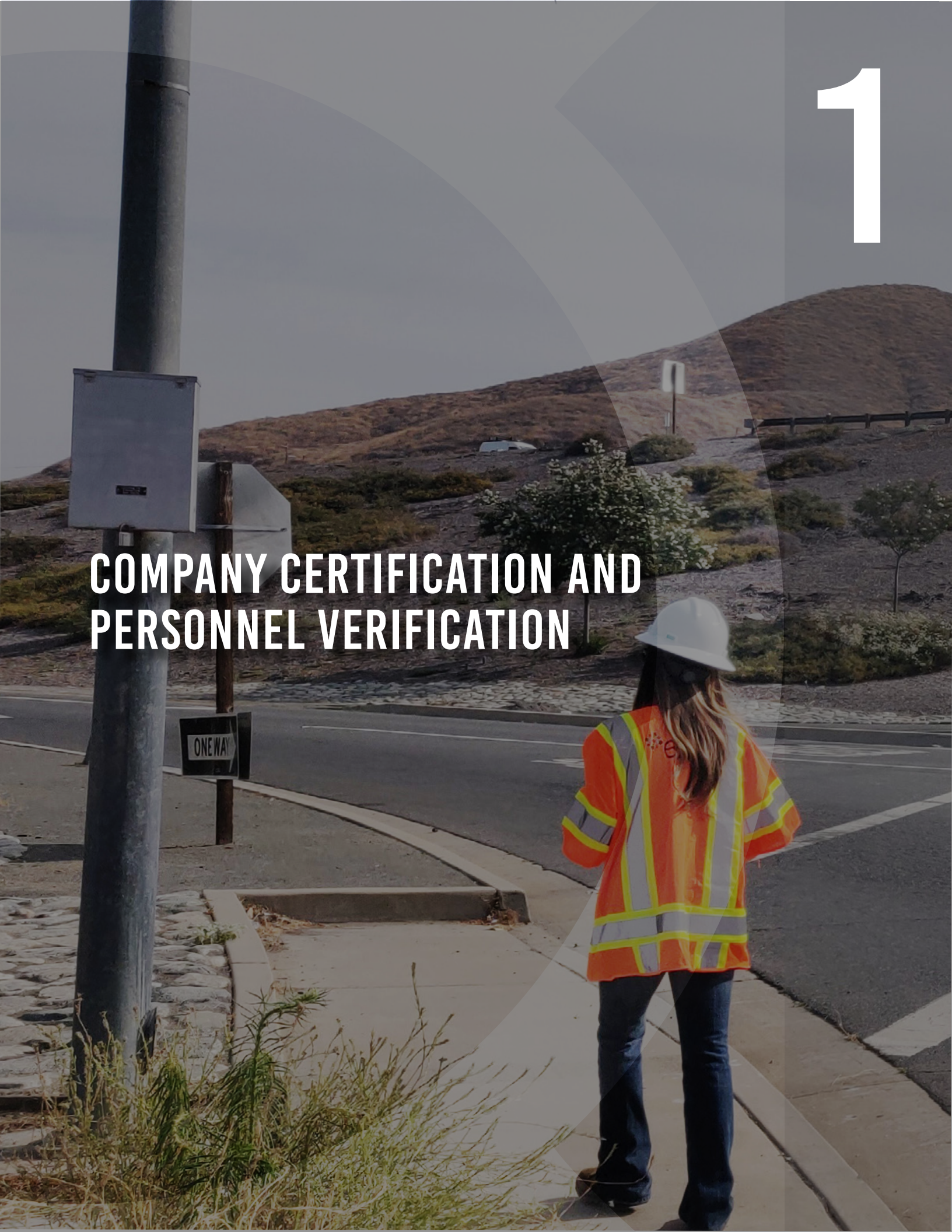
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COMPANY CERTIFICATION AND PERSONNEL VERIFICATION





December 2, 2021

Public Works Department
City of Carson
701 East Carson Street
Carson, CA 90745

SUBMITTED VIA: The City of Carson's Planet Bids Portal

RE: Request For Proposal 21-040 | Construction Management Services for Project No. 675 - Sepulveda Boulevard Widening Project

To The City of Carson,

EXP is pleased to submit our proposal to the City of Carson for Construction Management Services for the Sepulveda Boulevard Widening Project.

With a mission to understand, innovate, partner and deliver, EXP provides engineering, architecture, design and consulting services to the world's built and natural environments. EXP has 90+ offices in North America, including 30+ offices across the United States, and five offices in California, including San Bernardino, Glendale, Los Angeles, San Diego and San Francisco. As a large multidisciplinary firm, we offer significant in-house technical expertise in relevant areas including roadway design, structural/bridges, drainage, civil, traffic, and construction management. EXP is consistently recognized by our clients for the quality of services and deliverables we provide. We stand for making great things happen.



EXP has assembled a strong and locally experienced team of professionals led by our **PROJECT MANAGER / RESIDENT ENGINEER / STRUCTURE REPRESENTATIVE (PM/RE/SR), ANH CASE PE, CCM**. Anh has served as Project Manager on a wide variety of municipal and transportation agency projects large and small. She brings 23 years of professional experience in project management and construction management. She is a hands on, knowledgeable, and highly respected project manager and Resident Engineer/Structures Representative who is intimately familiar with Caltrans specifications and standards as well as the Greenbook construction requirements. Her experience includes a wide variety of bridge construction over waterways such as the Atwood Channel, Santa Ana River, Santiago Creek, and San Marcos Creek where she worked closely with the US Army Corps of Engineers (USACE), US Department of Fish & Wildlife (USDFW), California State Water Resources Control Board (SWRCB) and Orange County Flood Control District (OCFCD). She also understands the importance of leading by example, resolving project issues, building consensus, and partnering with the contractor to get things done. Anh possesses strong technical and leadership skills and has a proven track record of successfully delivering projects. EXP commits that Anh and every member of our team will embrace the following principles throughout the development and delivery of the project.

**ANH HAS DELIVERED
NUMEROUS BRIDGE
PROJECTS THROUGHOUT
SOUTHERN CALIFORNIA
INCLUDING THE
BORDEN BRIDGE IN SAN
MARCOS, CA WHICH
WAS COMPLETED ON
TIME AND 10% BELOW
BUDGET DUE TO ANH'S
LEADERSHIP AND
PARTNERING EFFORTS
WITH THE CONTRACTOR.**

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COLLABORATE | EXP will work closely and collaboratively with the City of Carson and all stakeholders in a true team environment, to identify and resolve issues, to communicate effectively, to achieve consensus and to deliver this project successfully to the City of Carson and the stakeholders.

DELIVER QUALITY WORK | EXP will prepare and implement a rigorous project Quality Assurance Plan (QAP) and procedures to ensure that the EXP Team delivers the Sepulveda Bridge Widening Project with the highest quality.

COVID-19 PROTOCOL | Safety is EXP's number one priority. EXP has taken the necessary precautions to ensure the safety and well-being of our employees during these unprecedented times. EXP has developed a guideline and educated our staff on infection control protocols designed to minimize and eliminate the risk of infection for employees. We also require that our subcontractors provide company guidelines for dealing with COVID-19 as a part of this preparation.

Our proposal contains a comprehensive description of our professional qualifications; the personnel we propose to utilize, both EXP employees and our outstanding team of subconsultants; and a description of recent similar projects we have successfully managed.

CONTACT PERSON FOR ANY QUESTIONS RELATED TO THIS RFP | Anh Case, PE, CCM | Project Manager, Resident Engineer, and Structures Representative (PM/RE/SR) | t: 714.423.8171 | e: anh.case@exp.com.

ACKNOWLEDGMENTS

We acknowledge receipt of both Addendum No. 1 and one Q+A document issued for this RFP, and our proposal shall remain valid for a period of not less than 90 days from today's date. ***We confirm that EXP has never been terminated from a contract.***

Our signature on this letter attests that all information submitted in this proposal is true and correct and that ***the person signing this letter is an officer of EXP and is authorized to bind the firm*** to the terms of the proposal. ***We also certify that we are in good standing with the California Secretary of State (entity # C3365360).*** We look forward to the opportunity to discuss our team's qualifications with you in the near future. Together, let's explore the possibilities.

Respectfully Submitted,

A handwritten signature in black ink, appearing to read 'Khalil Saba'.

Khalil Saba, PE
Principal-In-Charge

1000+

bridges widened, inspected,
planned, rehabilitated, designed,
and constructed

100+

year history in highway and
roadway projects

#18

Top 20 Design Firms
in Transportation
ENR, 2021

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SUBCONTRACTOR LIST



SUBCONTRACTORS: COMPANY HISTORY + INFORMATION



The EXP team's approach is to include a team of subcontractors with the expertise and experience best suited to successfully deliver the scope of work described in the RFP. We have selected firms who are local to the Carson area, are familiar with the project site, and have existing working relationships in the area and who have a solid record of delivering projects with similar challenges and will meet the City's MBE/WBE/DBE goal.

SURVEY | CHAUDHARY + ASSOCIATES INC. (C&A) | DBE

Chaudhary & Associates, Inc. is a California Corporation and State of California certified **DBE**, SBE, LA County Community Business Enterprise, and Regional Business Enterprise (RBE) firm. C&A has been in business for 40 years. They have established a reputation providing civil engineering, land surveying, and construction management services to public works clients throughout the state. C&A has provided mapping, surveying, and right-of-way engineering services on more than 1,200 public works projects such as highways, railroads, pipelines, port facilities, bridges, airports, transit, utilities, and roadways throughout California. Their history in providing services is a testament to the financial, technical, and corporate stability and continuity of the firm. C&A will be servicing this project out of our Long Beach office.

C&A currently holds two contracts with the County of Los Angeles and has been providing civil engineering and surveying services throughout California for the last 40 years. The professional staff of 11 Professional Land Surveyors and 5 Registered Civil Engineers forms the nucleus of the firm, backed by a team of more than 40 associate engineers and surveyors, field survey crew staff, inspectors, technicians, and other support personnel with additional qualified on-call staff available. The staff level at C&A has been consistent and reliable over the course of the life of the firm. Currently there are four staff with 30+ years with the firm, six staff with 20+ years, and ten with 10+ years.

LABOR COMPLIANCE | GCAP SERVICES (GCAP) | DBE

GCAP is a southern California-based professional consulting firm with over 23 years of labor compliance experience for transit and transportation related projects, and is a certified **DBE**. As a leader in the development and implementation of compliance programs and systems, GCAP possesses significant experience providing labor compliance services for transit and transportation related projects. Their team possesses experience providing labor compliance monitoring on projects of all sizes ranging from weeklong engagements to multi-year billion-dollar transit and highway projects. We understand the nuances of each type of project and how to tailor their support based on each project's unique needs.

Since its founding, GCAP has successfully delivered over 180 high-quality consulting engagements to federal, state, county, and local government agencies. The GCAP team is experienced at providing labor compliance guidance to agencies, primes, and subcontractors in accordance with Caltrans Local Agency Procurement Manual (LAPM). This includes pre-construction conferences and a variety of labor compliance responsibilities including all aspects of compliance, such as reviewing certified payroll submittals, confirming all fringe benefit and apprentice requirements are met, conducting jobsite interviews, and reviewing daily reports to ensure all labor compliance documentation is accurate and complete. Their team has implemented the LCPTracker application for various clients and has extensive experience providing monitoring services utilizing the LCPTracker platform. In addition to their strong technology-oriented background, they maintain our expertise at conducting hard copy audits of certified payrolls.

MATERIALS TESTING + SOURCE INSPECTION | HAMILTON & ASSOCIATES INC. (H&A) | SBE

Founded in 2004, Hamilton & Associates is a seasoned engineering company that provides a full spectrum of geotechnical engineering, materials testing, and inspection services. H&A performs site investigations and specialty geotechnical studies at feasibility-level planning, design and construction stages.

H&A's geomechanics laboratory, located in Torrance, California is well equipped and staffed with certified technicians for performing soil and concrete tests, per ASTM, AASHTO, and Caltrans.

ENVIRONMENTAL SERVICES | VCS ENVIRONMENTAL (VCS) | DBE/WBE

VCS Environmental is a full-service consulting firm specializing in the building and transportation industries across Southern California. They specialize in planning, biological, cultural, regulatory, and geospatial services. VCS has over 25 years of experience providing environmental consulting services for their clients.

VCS is a **DBE and woman-owned business** with extensive experience working with public agencies and private firms to assist them in meeting project goals. Their approach is one of advocacy, where they assist their clients in identifying the best strategies while trouble-shooting to minimize risk and liability. Their model allows them to be nimble, meet deadlines more effectively, and pivot quickly when situations dictate a fast response. VCS has a highly focused team of professionals with decades of experience and are committed to enriching the communities in which they serve through their mentorship and grant programs.

VCS has been fortunate to serve a myriad of public and private sector clients, working on jobs ranging from smaller development projects to \$1.8 billion transportation improvements. Their clients include builders/developers, cities, counties, water districts, school districts, the State of California, Caltrans, and Metropolitan Planning Organizations throughout Southern California.



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PROJECT UNDERSTANDING, APPROACH AND PRACTICE

3 PROJECT UNDERSTANDING, APPROACH AND PRACTICE

PHASES OF WORK

PRE-CONSTRUCTION SERVICES

CONSTRUCTION BIDDING | EXP will assist the City of Carson in all phases of construction of the Sepulveda Bridge Widening project starting with assisting in construction bidding. **Anh Case, our Project Manager/Resident Engineer/Structures Representative (PM/RE/SR)** will utilize a Constructability Matrix to consolidate all the comments and responses generated from the contract plans and specification review. We will also verify the Engineer's Estimate for accuracy and provide any value engineering analysis. A Constructability Review of any project prior to Ready-To-List is crucial in developing a biddable and buildable project. In preparation for this RFP, our team has reviewed the plans and specifications and began a Constructability Matrix (Exhibit 1 on the next page) for the project.

BID SUPPORT | Our team will attend the Pre-bid meeting and answer questions submitted by prospective bidders and prepare addendum/addenda as needed. Once bids are received, EXP will perform a bid analysis and assess the most responsive bidder and determine the lowest bidder by use of a Bid Summary. We will also assist the City in the preparation of the contract award package and agenda items for City Council meetings.

PRE-CONSTRUCTION MEETING | Once the contract has been awarded, we will prepare the Pre-construction Meeting agenda and invite all stakeholders such as the City of Carson, Army Corps of Engineers (USACE), Los Angeles County Flood Control District (LACFCD), U.S. Fish and Wildlife Service (USDFW), State Water Resources Control Board (SWRCB), and other utility and pipeline owners. We will assist the City with answers to questions and follow up with meeting minutes and action items. Our team will engage all stakeholders and keep them informed on all aspects of work. Topics for discussion at the pre-construction meeting include contract requirements, existing conditions of the project, expectations of the EXP CM Team, requests for the contractor's work schedule, regulatory permits, NPDES requirements, and utilities. **Tim Kirkley, our Office Engineer (OE)**, will prepare and disseminate minutes of the pre-construction meeting to all participants to memorialize the takeaways from the meeting.

NTP + CONTRACTOR COORDINATION | Upon issuing the Notice to Proceed to the contractor, the EXP team will begin coordination with the contractor on a daily basis. **Ju Kim, our Senior Inspector**, will coordinate with the contractor, along with any subcontractors, on field work activities.

EXISTING SITE CONDITIONS DOCUMENTATION | Prior to the start of construction, our team will document the existing project site conditions through reports, videos, and photographic evidence. Any existing items not shown correctly on the project plans will be noted and documented. We will assist the City and contractor in coordination and processing utility service applications, utility relocations, and connections before and during construction and assist in permitting and coordination with LACFCD, USACE, USDFW, SWRCB, SCE as well as all other utility owners affected by the project.

EXHIBIT 1 | CONSTRUCTABILITY MATRIX

ITEM NO.	DWG. NO. / SPEC. SECT. PAGE	DISCIPLINE	INIT.	COMMENT	COST IMPACTS	NO. COST IMPACTS	SUGGESTED CORRECTION
1	TP	Structures	EXP	There appears to be cracks on bridge decks. There does not appear to be a Spec section or bid item to address the repair for the cracks.	●		Add Methacrylate section to Specials and bid item to address cracks on existing bridge deck.
2	pg. TP-34	Spec. Provisions	EXP	"Payment for drilling and doweling into existing driveway approach is included in the unit price for P.C.C. driveway approach."	●		Need drawing showing drill and bond details.
3	Plan Sheet C-7	Roadway	EXP	Wrong call-out for Construction Note 7.		●	Use correct call-out shown on plan C-4.
4	Plan Sheet C-4	Roadway	EXP	Wrong call-out for joint Driveway Detail.		●	Use call-out shown No. 7, not 58.
5	pg. TP- 9	Spec. Provisions	EXP	Pay item 41, 42, 43, 44, currently not called out as Final Pay.		●	Identify as Final Pay.
6	pg. TP- 50-53	Spec. Provisions	EXP	Bridge Removal and protective cover shall be designed by P.E.		●	Add P.E Requirement.
7	pg. TP - 70-72	Spec. Provisions	EXP	Need to mention no diesel hammer shall be used over channel.	●		Add no diesel hammer will be allowed over the channel.
8	pg. TP - 48-50	Spec. Provisions	EXP	Coupler should be from Caltrans pre-approved material list.		●	Use Caltrans Pre Approved List.

SAFETY

Safety is a leading core principle and key to our culture at EXP. We have a firm belief that all incidents and accidents are avoidable. Our team will engage construction staff and public through out the duration of the project to continually improve safety by implementing the following action items:

- **SAFETY TAILGATE MEETINGS.** We will participate in the required meeting every 10 working days and review the contractor's upcoming construction activities, including Job Hazards Analysis.
- **CONTRACTOR'S DAILY SAFETY BRIEFING.** We will actively participate in the contractor's daily safety briefing to understand and communicate safety hazards associated with daily activities.
- **FIELD PERSONNEL TRAINING.** All EXP Staff and any staff that comes to the project have the 10-hour Cal OSHA safety training and defensive driving.

We will review and approve the contractor's safety plan and observe work activities for compliance with City, State, Construction Safety Orders, Cal-OSHA standards, and the Enhanced Electrical Safety Policy. Although the contractor is responsible for implementing its safety plan, monitoring its workforce, and correcting deviations, we will immediately notify the contractor and you of any observed deviations from the safety plan. As required, we will recommend the corrective actions needed. A discussion of safety is a standing item at the weekly construction progress meetings.

COVID-19 | EXP has taken the necessary precautions to ensure the safety and well-being of our employees during these unprecedented times. For on-site staff, EXP ensures that all employees are equipped with the right personal protective equipment, such as masks, gloves, and adherence to physical distancing measures, where possible. EXP has also developed and educated our staff on infection control protocols designed to minimize and eliminate the risk of infection for all employees.

During the current COVID-19 pandemic, EXP has demonstrated our ability to adapt to changing times and our agility in decision-making. We have adapted to every situation with little to no disruption to ongoing projects and continued to deliver exceptional consulting services to our clients.

CONSTRUCTION MANAGEMENT

WEEKLY CONSTRUCTION PROGRESS | Anh will conduct weekly construction progress meetings with the contractor, designer, City representatives, utility owners, and other stakeholders to address safety issues, violations of contract procedures, actual and potential problems, the status of RFIs, submittals, contract change orders, review the project schedule and work activities. Anh will provide the Weekly Statement of Working Days

(WSWD) to the contractor every Monday morning to capture the previous week's work and running total of working days used to date as well as any impact to the current construction schedule. Tim will prepare the meeting agenda for the facilitation of the meeting, keep discussions focused and send the meeting minutes within 72 hours.

DOCUMENT CONTROL

The EXP team will maintain project documents in accordance with the document control procedures and processes established in the Caltrans Construction Manual (63 Categories). We will verify record management conforms to MTA reporting (quarterly) requirements for reimbursement and as approved by the City's Project Manager. We will maintain hard copies and electronic files using Adobe PDF format for all documents including correspondence, an emergency contact list, contract drawings, submittals, RFI's, shop drawings, As-built record drawings, standard forms, and reports. All electronic files will be housed in a cloud-based system, such as SharePoint. Access will be given to the City and any other necessary stakeholders. This will allow the City to access the documents 24/7 at their convenience. We will develop and maintain project logs, project reports, progress payments, contract change orders, and other project correspondence. In addition, our team will document the project with pre-construction photos and videos of the existing conditions, progress photos of the work, completed construction items with location, date, and activity. Logs will be developed and maintained throughout the life of the project to track various contract-related items such as: Correspondence, Submittals, RFI's, CCO's, Materials testing and sampling, Survey Requests, Public Relations/Communications. This attention to detail ensure a smooth auditing process per the Caltrans LAPM. Our filing system will also include a "Potential Claim" file with all related documents. We will notify the City's Project Manager of the issue and provide a detailed explanation of the claim and proposed resolution alternatives with a recommendation as to the preferred solution.

SUBMITTALS AND RFIS | We log, file, and monitor all contractor submittals, material, and equipment certifications, and shop drawings per the Caltrans Construction Manual, Caltrans Specifications, Greenbook, City Standard Details and the Caltrans Bridge Records and Procedures Manual. We status each submittal as it is received, accepted, returned for resubmittal so that the party responsible can be identified. Due dates of pending shop drawings are always known. We apply the same procedures to control the RFI process. Any submittals or RFIs requiring third-party concurrence are noted as such in the log and routed to the appropriate party as needed for their review, comment, or concurrence.

TOP 10 SUBMITTALS

- SWPPP Plan
- CPM Baseline Schedule
- Traffic Control Plan
- Shoring Plan
- Pile Installation Plan
- Pre-Cast Pre-Stressed Girder Shop plan
- Bridge Jacking Plan
- Pre-Cast Quality Control Plan
- Concrete Mix Design + AC Mix Design
- Long Lead Materials Purchase Request

DAILY REPORTS | During construction, Anh and Ju will generate a Daily Reports to include; date, day of the week, weather, contractor workforce and equipment, item work being performed with quantities placed, controlling item of work, extra work, field memos, notice to the contractor, labor compliance, differing site conditions, and unusual occurrences, visitors, utility information, conversations with stakeholders and contractor, accidents, encountered safety issues and resolution, photographs, inspection and testing performed, and regulatory and materials compliance. Inspections shall include contractor's safe work practices, traffic control and closures, and compliance with SWPPP and other project-related permits. As part of our team, specialty inspections, such as biologist, will be provided on an as-needed basis to assure the City that every aspect of contract work will be inspected by professional-level personnel throughout construction.

ACCEPTANCE TESTING AND SOURCE INSPECTION

Ju will coordinate the testing and sampling of concrete batch plant, source inspection for the pre-cast girders and piles, subgrade soil, aggregate base, concrete, masonry, Hot Mix Asphalt (HMA). Testing of materials will be performed by Hamilton & Associates per the project plans, specifications, LAPM, and Caltrans QAP and shall include soil, aggregate base, structural and minor concrete, and HMA. During construction, Ju will meet with Anh to discuss the day's operations. Materials will be sampled and tested at the established frequencies at our laboratory facility. Should a non-compliant issue arise, Anh will notify the City by email and letter. The test results for each material will be processed into a standard report format and sent to Anh in the timeline established in the Caltrans QAP. A summary of all acceptance testing will be maintained and updated throughout the project. The summary will include the date, test method, location, test result, specification limit, and action taken if the result has failed. A follow up meeting will be held to discuss lessons learned and how to avoid similar non-conformance.

SURVEY

Chaudhary & Associates (Chaudhary) will support the EXP team by providing construction surveying services for the Sepulveda Boulevard Widening Project. Chaudhary was invited to join our team because of their vast experience in construction staking and monument perpetuation using the Caltrans Survey Manual as a primary reference source.

Chaudhary takes a proactive approach working with construction management teams by working closely to help identify constructability issues and assisting with finding solutions. This proactive approach has historically allowed for the reduction in the number of change orders while helping the project to stay on schedule and within budget. EXP understands level of survey support will vary throughout the duration of the project so Chaudhary will maintain continuity of survey staff throughout each phase of the project, providing the project with surveyors who are familiar with the requirements and needs of the project.

Prior to start of construction, Chaudhary will review the survey project files including control, right of way maps, legal descriptions and plats, design files, and improvement plans as they pertain to the project.

Chaudhary will field-verify the horizontal and vertical project control for accuracy. Chaudhary understands the established project survey control provided by the City is the basis for all surveying to be performed. Any supplemental survey control will be adjusted to the designated control values.

Chaudhary understands Anh will be their primary contact and the only person authorized to approve contractor staking requests. The request process involves the contractor submitting a written request for staking with enough advanced notice followed by Anh verifying the area is ready for staking. Anh will approve the request and forwards it to Chaudhary who will then schedule the work and prepares a field staking package which would include checking for most current plans or revisions, grade sheets, control, etc. Once the field crew completes the staking request, the party chief signs completes the request form and returns with the required copies of field notes to Anh. All original field notes and data collected shall be submitted to the City at the completion of the project.

Before, during and after construction, it is important to perpetuate any existing survey monumentation per the California Business and Professions code. Chaudhary shall provide these services as needed throughout the construction process, from preparing corner records to document existing monumentation to preparing a final Record of Survey to new right of way lines and/or new monumentation for the project in compliance with the California Business and Professions Code.

ENVIRONMENTAL COMPLIANCE

VCS Environmental (VCS) will assist EXP with environmental oversight and regulatory permitting related to the Sepulveda Boulevard widening, which is expected to include impacts to the Dominguez Channel.

VCS will work with EXP to confirm that environmental commitments described in the CEQA/NEPA document as well as mitigation measures from the regulatory permits are incorporated into the Plans, Specifications & Estimates bid documents.

VCS will obtain regulatory permits from the U.S. Army Corps of Engineers, California Department of Fish & Wildlife and Los Angeles Regional Water Quality Control Board as needed based on the project. Working in the Dominguez Channel, regulatory permits are still required if construction is within the ordinary high water mark of the channel and/or bed and bank, but we will advocate that the project is self-mitigating and no other mitigation is required. The EXP team will work with the City of Carson to determine whether project design can be modified to reduce or eliminate regulatory requirements as part of the value engineering analysis of the project.

VCS will oversee the contractor's biologist and/or cultural staff (if monitoring is necessary) to ensure we meet and exceed environmental commitments for the project.

CHANGE MANAGEMENT

The EXP team has extensive construction management experience dealing with contractors' requests for change and notice of potential claims. The process is to review, determine merit, and then firmly, fairly, and expeditiously resolve the change order issues in the best interests of the project with the City's support. Request for change orders must be discussed and shall obtain approval from the City's Project Manager prior to implementation. Should the work in question be encountered with no resolution, Anh will inform the City and activity will be tracked in the field as extra work and be included in the inspector's daily reports with the city's support

If a potential contract change is identified, we will initiate conversations with the City's Project Manager to clarify the issue in question and determine the impacts on the project. Anh will request a cost proposal from the contractor to ascertain validity, merit, and appropriate costs while concurrently performing an independent cost analysis. Once an agreement has been reached with the contractor, she will provide documentation to assist the City's Project Manager for approval. The project files shall contain all correspondence leading up to the execution of any Change Order and reference-related RFI/RFP includes detailed backup information.

Progress Pay Estimates to the contractor will be generated on a monthly basis, in cooperation with the contractor and approval by the City's Project Manager, to assure payment is made on agreed-upon bid item quantities incorporated into the work.

LABOR COMPLIANCE

Our team provides equal employment opportunity (EEO)/ labor compliance services during construction. At the pre-construction conference, Anh will provide to the contractor labor compliance documentation outlining their requirements per the contract. After commencement of construction, we will review certified payrolls and fringe benefit statements to make sure employees are paid the prevailing wage rate while maintaining copies of the certified payrolls in the project files. Ju will conduct random and periodic EEO/labor compliance interviews which Anh will review and sign. Issues related to EEO/labor compliance will promptly be brought to your attention. We will then notify you of unresolved issues and provide recommended courses of action to take. In addition, we will verify and document that the contractor is on track with its DBE goal monthly. Anh will also ensure the contractor adheres to the Enhanced Electrical Safety Policy and local hire provisions of the contract.

PARTNERING

Our team has extensive experience in formal and informal partnering. We will make sure the partnering accommodates all the stakeholders, including the City, the contractor, and all members of the construction management team. By encouraging a formal partnering relationship, we maintain an open line of communication with the contractor. By doing so, all parties understand each other's priorities and goals and work to the common objective of delivering the project successfully.



PARTNERING | PROVEN PERFORMANCE

On the OCTA I-405 Improvements Project, Anh and Ju actively scheduled and participated in regular follow-up partnering sessions that resulted in shorter submittal review periods.

MONTHLY PROGRESS REPORT

Anh will communicate weekly, with the City's Project Manager to discuss project status and any related issues. In addition, we will provide monthly project updates which will include:

- Work performed for the month
- Upcoming operations
- Monthly pay estimate with running bid item totals
- Contract Change Orders listing expenditures; approved and pending
- Notice of Potential Claims
- Project photos

Our monthly invoice will include an update to the budget with our plan on how to manage the project through completion while staying within budget. This will allow Anh to track expenditures and allow adjustments while maintaining the project budget.

We will assist the City by attending City Council meetings at construction contract award for the duration of the contract and as needed to help keep the Council apprised of the project.

PUBLIC RELATIONS

The EXP Team will work closely with the City of Carson and assist the City to establish a formal method of streamlining advance information to the public and addressing the concerns of the public and the media during construction. With the City's approval we will provide electronic and/or hard copy notifications and project fact sheets for distribution to the surrounding businesses. We also maintain a public communications log to ensure every concern is addressed. Under your guidance, we take a proactive role in addressing the public's issues. We are also available to attend community outreach meetings upon your request.



PUBLIC RELATIONS | PROVEN PERFORMANCE

For the controversial City of San Marcos Borden Road Bridge Improvements Project, *Anh delivered efficiently and effectively with minimal impacts to the adjacent residents and businesses.* Soon after the start of the project, she gained the trust of the residence of Royal Oaks Senior Apartments and continuously communicated with their residence to ensure they were kept abreast of any impacts to their community.

PROJECT CLOSE-OUT

PUNCH LIST | Prior to the last day of construction, our team will begin preparing an initial punch list for each item of work and consolidate the initial comments into a final punch list and share them with the contractor. We use a punch list log to identify problems and deficiencies in advance. This punch list log allows an opportunity to take care of issues in advance, prior to the project closeout stage. This process facilitates closeout efforts, making sure problems and deficiencies are identified and resolved early.

DELIVER FINAL AS-BUILT DRAWINGS | During the course of the project, we will maintain project as-built drawings showing all change orders and revisions as a result of responses to RFIs and CCO's. Our team will verify that the Contractor also maintains a set of updated as-built drawings throughout the life of the project. Our red-lined as-built set will then be provided to the Designer of Record for the preparation of final as-built drawings.

COORDINATE FINAL WALK-THROUGH WITH LOCAL STAKEHOLDERS | As the project progresses to the final six months of construction, we conduct weekly project coordination meetings with the designer, the City, utility owners, and other interested parties to discuss close-out of issues, noncompliance of contract procedures, and review the remaining project schedule and work activities. We facilitate the meeting, set the agenda, keep discussions focused and give all project participants an opportunity to update the team on construction issues within their jurisdiction. This results in a turnkey project with a smooth hand-off to the City.

FINAL COMPLETION REPORT | We maintain the project records in accordance with City procedures, Caltrans Construction Manual, and the Caltrans LAPM. Before the final completion reports are due, the Quality Assurance (QA) acceptance documentation is reviewed by our staff and City Project Manager to verify that files are complete. Once the files are deemed to be complete and all materials are acceptable, the materials certification form is signed and dated, noting any materials accepted that differ from the contract requirements.

DELIVERY OF PROJECT FILES

When construction is completed, Anh initiates assembly of the project history files, both electronically and hard copy, by transmitting designated records to the City Engineering Division.

COMPLETE CONSTRUCTION MANAGEMENT SERVICES WITHIN TWO MONTHS OF PROJECT CLOSEOUT

Our construction management team maintains a proactive approach in every assignment we take on, and that is the case as we manage the Sepulveda Bridge Widening Project. With this goal in mind, we are committed to closeout all construction management services, including the final release of project permits, within two months of the project closeout since we have full intention of staying ahead of all the activities described above. All reports are processed ahead of time. With effective use of partnering, we intend to deliver the project with zero claims. We will maintain close contact with all project partners during construction so as work is completed, we can receive "sign-offs" from all stakeholders. This proactive approach results in the project being completed on time.

KEY ISSUES MAP

WORK NOT IDENTIFIED IN THE RFP: During three recent visits to the project site we have identified some issues that may not have been addressed in the design phase of the project. We do understand that the Specifications are still being updated, at the time of this RFP, and that the project plans and specs have been shelved since 2013 and therefore, some of these issues may not have been in existence at that time. We have compiled our findings and potential solutions in the Issues Map below.



1 BRIDGE DECK CRACKS

SOLUTION: TREAT DECK CRACKS WITH METHACRYLATE

THIS WORK CAN BE DONE ONCE WIDENING IS COMPLETED AND TRAFFIC SWITCHED OVER TO THE OUTSIDE.

2 CONCRETE SPALLED AT JOINTS

SOLUTION: REMOVE UNSOUND CONCRETE AND CLEAN THEN REPAIR WITH POLYESTER CONCRETE

THIS WORK CAN BE DONE ONCE WIDENING IS COMPLETED AND TRAFFIC SWITCHED

3 BRIDGE DECK SPALLED

SOLUTION: REMOVE UNSOUND CONCRETE AND CLEAN THEN REPAIR SPALL WITH POLYESTER CONCRETE

THIS WORK CAN BE DONE ONCE WIDENING IS COMPLETED AND TRAFFIC SWITCHED OVER TO THE OUTSIDE

4 POTENTIAL CRACKING OF GIRDERS

UPPER SECTION OF THE PRE-CAST GIRDER, NEED TO INVESTIGATE TO DETERMINE IF THIS IS CRACK OR STAINED AT THE BOTTOM OF THE PRE-CAST GIRDER SPALL.

SOLUTION: INSPECT AND ENSURE THAT THE SPALL IS NOT AFFECTING THE STRUCTURAL INTEGRITY OF THE GIRDER. THIS WORK CAN BE INSPECTED AND REPAIRED DURING BENT CAP WIDENING

WORK PLAN

STRUCTURES APPROACH

The EXP team's experience managing and inspecting work on similar projects, provides valuable lessons learned to the Sepulveda Boulevard Bridge Widening over Dominquez Channel construction project. **Members of our team Anh Case, Ju Kim, and Tim Kirkley have worked on San Francisco Bay Bridge, San Diego Coronado Bridge as well as over the Santa Ana River, Atwood Channel and various channel projects throughout California.**

The project consists of the widening of the four spans pre-cast girder bridge to the north (westbound) and south (eastbound) of the existing bridge and retrofitting of the existing abutments shear key, widening the Bents caps, and replacing rocker bearing with elastomeric bearing pad. Bents 2, 3, and 4 are over the channel. The new section of the widening is four-span pre-cast girder bridges supported by sixteen 60" diameter CIDH pile foundations at Abutments 1 and 5 and twenty-four 16" driven battered piles at Bents 2, 3, and 4. The bridge length of 239'-11" ft is not affected by the widening. Widening the bridge by 20'-7" to the north and south, will allow for three lanes of traffic East and West-bound. The bridge width will be 96'-0" at the completion of the project.

After carefully reviewing the bridge plans and specifications, we will assess the two methods of approach for the Sepulveda Boulevard Bridge construction.

WORKER / CITIZEN SAFETY

Currently, there are encampments of unhoused individuals occupying under the bridge at Abutments 1 and 5. The first order of work is to work with the City of Carson, County of Los Angeles and LAHSA - Los Angeles Homeless Services Authority, to give proper notice to the unhoused individuals and relocate them away from the bridge with their belongings. A hazmat team may need to be present during the clean up to ensure that the area is cleared of any material or substance to ensure safety for construction workers. Once this task is completed, we will work with the contractor to secure the areas to prevent unhoused individuals from returning to encamp under the bridge.



1

FIRST APPROACH OPTION | WIDEN THE NORTH WESTBOUND (WB) AND SOUTH EASTBOUND (EB) SIDE AND BRIDGE RETROFIT SIMULTANEOUSLY

Working on all three areas of the bridge simultaneously can increase efficiency, reduce construction schedule, and decrease construction cost. This operation will require reducing traffic to one lane in each direction and closing of the pedestrian walkway in both directions, allowing the contractor to work on both sides of the widening as well as working on the existing bridge retrofit simultaneously. One major obstacle to this method will require the existing overhead power line currently located on north and south sides to be relocated away from the affected areas of the construction. With this option, below are the 3 stages we anticipate for the construction of the bridge.



STAGE 1: WIDEN WESTBOUND (WB) AND EASTBOUND (EB)

STAGE 1 | WIDEN NORTH (WB) AND SOUTH (EB) SIDE AND BRIDGE RETROFIT SIMULTANEOUSLY

- Relocate SCE power lines on north (WB) and south (EB) side to drive bridge piles and Cast In Drilled Hole (CIDH) pile operation
- Reduce lanes to one lane each direction
- Close pedestrian walkways
- Set K-Rail on WB and EB side
- Remove sidewalk and bridge overhangs from both sides of the bridge
- At Bents 2, 3 and 4 drive piles on one side of the bridge then continue version the other side
- Install shoring at Abutments 1 and 5 on WB and EB side
- Excavate at Abutments 1 and 5 on WB and EB side
- Construct CIDH piles at Abutments 1 and 5 on WB and EB side
- Widen Bents 2, 3 & 4 caps on WB and EB side
- Construct Abutments 1 and 5 widening
- Set Pre-cast Pre-tressed (PCPS) bridge on WB and EB side
- Place bridge deck concrete on WB and EB side
- Place closure pour concrete on WB and EB side
- Construct sidewalk and barrier rail on WB and EB side
- Retrofit existing abutment shear key, jack bridge and replace rocker bearing

STAGE 2 | INSTALL CIDH BEHIND ABUT 5 (MIDDLE)

- Move traffic to outside lane (new widened sections)
- Install 2 CIDH piles behind the Abutment 5 (middle)

STAGE 3 | ROADWAY IMPROVEMENTS

- Construct Retaining Walls 1 and 2
- Install Storm Drain and Catch Basins
- Construct Private Driveways, Curb, Gutter and Sidewalk
- Paving
 - Place traffic on either side of roadway
 - Place new pavement on opposite side of roadway
 - Repeat process for other side of roadway
- Install/Improve Traffic Signals, Street Lighting, Landscaping and Fencing
- Final Stripping

**2****SECOND APPROACH OPTION | WIDENING THE NORTH WESTBOUND (WB) AND SOUTH EASTBOUND (EB) SIDES SEPARATELY**

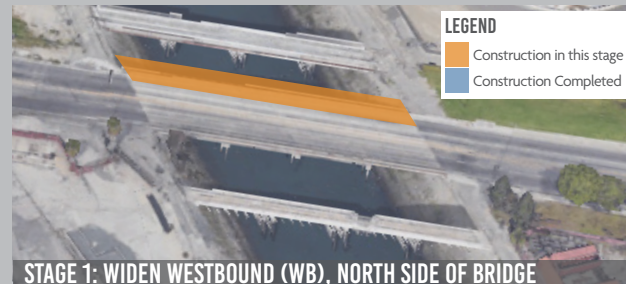
This method can be used to minimize impact to traffic and pedestrian. Keeping one side of the pedestrian access open during construction and keeping all lanes open during construction.

STAGE 1 | WIDEN NORTH (WB) SIDE AND BRIDGE RETROFIT

- Relocate SCE power lines on north (WB) and south (EB) side to drive bridge piles and Cast In Drilled Hole (CIDH) pile operation
- Shift traffic to south side
- Set K-Rail on WB side
- Remove WB sidewalk and bridge overhang
- At Bents 2, 3 and 4 drive bridge piles on the WB side
- Install shoring at Abutments 1 and 5 on WB side
- Excavate at Abutments 1 and 5 on WB side
- Construct CIDH piles Abutment 1 on WB side
- Widen Bents 2, 3 & 4 caps on WB side
- Construct Abutments 1 and 5 widening on WB side
- Set Pre-cast Pre-stressed (PCPS) bridge girders on WB side
- Retrofit existing abutment shear key, jack ridge and replace rocker bridge

STAGE 2 | WIDEN SOUTH (EB) SIDE

- Shift traffic to north side
- Set K-Rail on EB side
- Remove EB sidewalk and bridge overhang
- At Bents 2, 3 and 4 Drive bridge piles on the EB side
- Install shoring at Abutments 1 and 5 on EB side
- Excavate at Abutments 1 and 5 on EB side
- Construct CIDH piles Abut 1 and 5 on EB side
- Widen Bent 2, 3 & 4 caps on EB side
- Construct Abutments 1 and 5 widening on EB side
- Set PCPS bridge girders on EB side
- Pour bridge deck on EB side
- Pour closure pours on EB side
- Construct sidewalk and barrier rail on EB side



STAGE 3 | INSTALL CIDH AT ABUTMENT 5 (MIDDLE) AND ROADWAY IMPROVEMENTS

- Move traffic to outside lanes at Bridge only
- Install 2 CIDH piles behind the Abutment 5 (middle)
- Construct Retaining Walls 1 and 2
- Install Storm Drain and Catch Basins
- Construct Private Driveways, Curb, Gutter and Sidewalk
- Paving
 - Place traffic on either side of roadway
 - Place new pavement on opposite side of roadway
 - Repeat process for other side of roadway
- Install/Improve Traffic Signals, Street Lighting, Landscaping and Fencing
- Final Stripping



After reviewing the project plans, we have identified some of the challenges below:

ACCESSING THE DOMINQUEZ CHANNEL | Prior to construction, the contractor will be required to obtain a permit from the Los Angeles County Department of Public Works; operator of the Dominguez Channel. A rainy season moratorium extending from October 15th to April 15th prohibits any divergence of channel flow through the project. However, during the non-rainy season, the contractor may be allowed to divert a portion of flow to allow for work in the channel.

Pile driving equipment and cranes, due to weight issues, will not be able to position on top of the bridge for any activity, such as pile driving, and placement of the girders.

There are two methods of gaining access into the channel for equipment - a barge, or constructing a temporary truss system. Due to close proximities of the utility bridge on the North and South of Sepulveda Blvd bridge and not having the vertical clearance, a barge will not fit under the utility bridge with a crane or equipment. The barge will need to be brought empty, then the equipment loaded near the bridge abutments, creating a ramp. In lieu of the barge, building a temporary truss system to access the channel for pile driving and equipment can also be achieved. To build a truss system, we will need to work closely with Army Corp, the County of Los Angeles, and the City of Carson regarding permits and requirements.

BRIDGE DEMOLITION | During the removal of the existing bridge railing, sidewalk, wing walls, and portions of the bridge deck, and pile cap, Ju will make sure a protective cover is constructed to prevent any materials, equipment, or debris from falling into the Dominguez Channel. The protective cover requirement is a minimum of 5 feet beyond the side of the bridge.

PILE DRIVING | We do not anticipate issues, however, in pile driving operations two things can happen, soft driving or hard driving. For soft driving, we need to leave the pile high and perform a retap after 24 hours. If hard driving is encountered we can add a driving tip or cut the top of piles with designer approval.

BRIDGE OVER CHANNEL | PROVEN PERFORMANCE

One of Anh's previous assignments was on the OCTA Tustin-Rose Grade Separation Project over the Atwood Channel in Placentia, California. The project included construction of a new bridge over the channel operated by the Orange County Flood Control District. Anh worked with the contractor to ensure that the moratorium of work during the rainy season was adhered to.



Due to piles being batter, the pile hammer and fixed lead will be over a lane line. Knowing this, we will need to close a lane during pile driving operations. For safety concerns, we will not perform pile driving operations over live traffic.

Prior to pile driving operation, our team will require the items below for review and approval. In addition, there will be a pre-activity meeting to review the contractor's work plan and safety plan.

- Crane certification
- Operator certification
- Shop drawings
- Driving system submittal - Pile hammer information
- Contractor pile handling plans
- Contractor pile pick plans
- Drivability analysis -wave equation analysis stamped and signed by a PE in California
- Blow count per hammer stroke
- Traffic handling plan during pile driving operation

BEARING PAD RETROFIT: First order of work will be to widen the bent cap, which will increase the bent cap area. With the increased area of the cap, we can jack under the girders to raise the bridge. To replace the rocker bearing, the bridge will need to be raised/jacked in placement of support and remove the rocker bearing and cut the bearing pad out from under the girders.

The key to success for this operation is making sure we perform the following: reviewing and approving Contractor safety plans, working drawings, jacking plans, temporary support once rocker bearing is removed, containment plans, bent cap forming system. Prior to activity, we will have a pre-activity meeting to review the contractor's plan of action and safety.

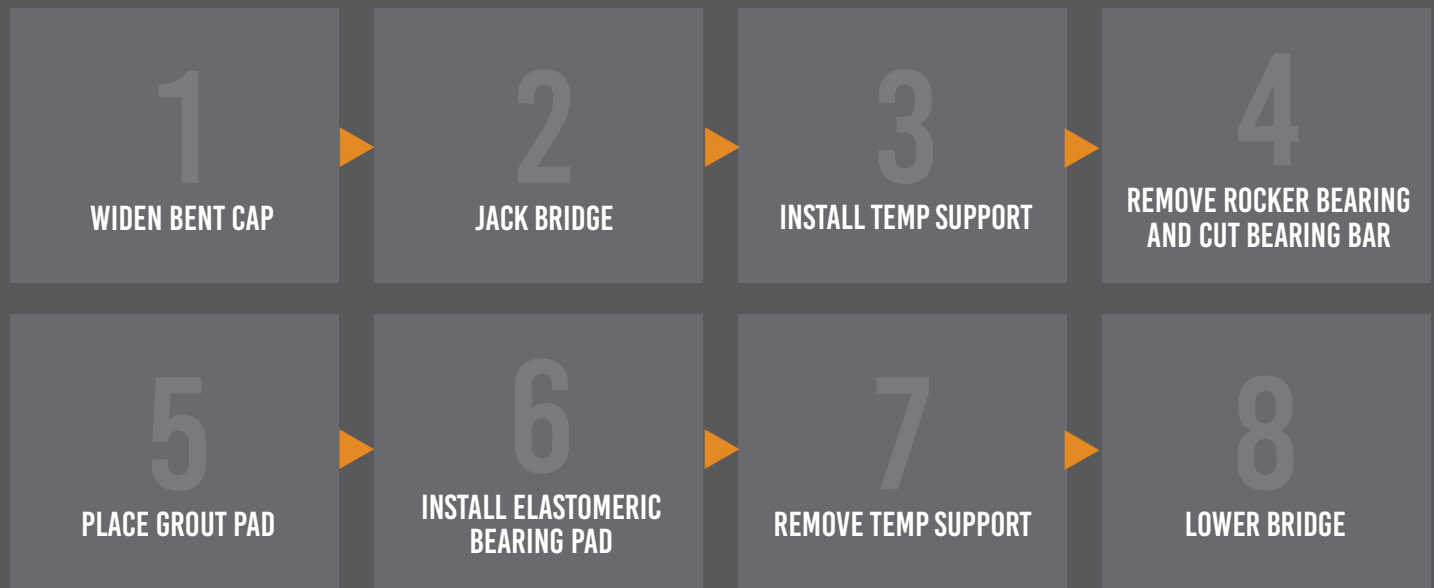


PILE DRIVING IN WATER



ELASTOMERIC BEARING PAD

STEP BY STEP FOR BEARING RETROFIT



CIDH: Since the specified Tip Elevation of the CIDH is much lower than the water table the piles will be constructed using the wet hole method “Slurry Displacement Method”. The groundwater elevation of -6.5 to -6.6, top of pile elevation of 8.0 ft, and the bottom of pile elevation of -40.0. Utilizing the slurry displacement method to maintain the drilled hole, will prevent cave-ins during drilling and keeping the hole intact during CIDH concrete pour. During the CIDH operation, our team will monitor the slurry for the following: testing of the slurry for PH, viscosity, density, and contents. Our team will be inspecting the diameter of the augur, plumbness of the hole, and measuring the depth during this operation. In addition, we will ensure that the contractor uses a clean-out bucket once finished drilling to clean the bottom of the piles. For the CIDH concrete placement, our team will check the concrete tremie pipe to ensure it maintains 10ft of concrete head from discharge. Ju will maintain a pour along the cubic yard of concrete going in and measure the CIDH height.

This log will identify any cave-ins during the concrete pour. Because the upper section of concrete is contaminated during the CIDH concrete pour with slurry, we will work with the contractor to vacuum or remove the top of CIDH concrete until we obtain acceptable concrete. Soon after the concrete pour, the inspection tube will be checked for plumbness and blockage, once this has been checked, we will fill the inspection tube with water preventing the inspection tube from separating from concrete during curing of the CIDH. These steps are important to perform testing using a Gamma Gamma test which utilized the inspection tubes, to ensure we obtained sound concrete throughout the length of the CIDH piles.

Prior to the CIDH operation, our office will require the following for review and approval. In addition, there will be a pre-activity meeting to review the contractor’s work plan and safety plan.

CIDH OPERATIONS CHECKLIST:

- ☑ Check pile layout
- ☑ Check elevation
- ☑ Check drilling equipment and augur size
- ☑ Check the bottom of the drilled hole is cleaned, using a clean-out bucket
- ☑ Check CIDH reinforcing steel
- ☑ Check inspection tub
- ☑ Check dobies for proper concrete coverage on the CIDH
- ☑ Check concrete ticket
- ☑ Check concrete temperature
- ☑ Check revolution of the concrete mix truck
- ☑ Check time on the concrete ticket
- ☑ Check concrete is in the center of the pile, prevent segregation.
- ☑ Check vibrator/tinger is properly used on the upper 15 ft of the pile
- ☑ Check curing compound is mixed and applied evenly



CIDH PILE REBAR CAGE



CIDH DRILLING OPERATION

Below are some of the critical plans and inspection items for the CIDH operation

- CIDH work plan
- CIDH placement plan
- CIDH pick plan
- Wet Hole
- Concrete Mix design
- Slurry displacement plan
- Gamma Gamma testing tube
- PH and viscosity testing
- Log for pour rate vs concrete level
- Clean out bucket

The following Certificates of Compliance will be collected and filed for CIDH:

- Reinforcing Steel
- Anchor bolts
- Concrete
- Curing Compound
- Inspection tube
- Slurry recycle plan

FALSEWORK: Erecting and maintaining falsework over the channel for the bent cap widenings and new bent cap construction will be challenging. For the bent cap widenings, a hanging forming system will be ideal for this type of construction. Using the existing bent cap to hang/suspend a platform for bent cap widenings can be an option for the contractor. Another option is for new bent, using a collar on the driven piles to support the forming system to construct the bent caps.

BRIDGE WIDENING SURVEYING AND DECK CONTOURS: A challenge on a bridge widening is to survey the existing deck and generating a profile for the widening section of deck profile for proper cross fall. Adjusting the new deck profile to match existing is a skill that comes from experience. Fine-tuning the new deck contour using existing deck survey is critical to prevent ponding and maintain proper cross fall. Our team members have performed many widenings and understand the complexity and importance of surveying the existing edge of the deck and generating new deck grade/contour. **Anh and Ju have provided deck widening profiles for over 30 bridges including the Santiago Creek Bridge Widening in Orange County and Beach Blvd. Bridge Widening on the I-405 Improvements Project.**

PRE-CAST GIRDER + PILES: The EXP Team have constructed many bridges with pre-cast girders including the Santiago Creek Bridges on the SR22 Improvements Project in Orange County. In addition, **Anh worked at pre-cast concrete facility fabricating and implementing the QC/QA process for pre-cast girders and piles.** Our team's vast experience working with pre-cast girders require no learning curve and we will hit the ground running from the submittal review to inspection to girder placement. For this 4-span bridge with pre-cast girders, our team will use the Caltrans Standard Specification, Section 50, Special Provisions, Caltrans Construction Record and Procedure Manual and Caltrans Pre-stressing manual to ensure quality.

Anh will review and approve the following Shop Drawings:

- Contractor Work Plan
- Pre-stress shop drawing
- Mix design for pre-cast girders and piles
- Pre-stress drawings for post tension
- Pre-cast Quality Control Plan
- Critical Lift Plan - designed by PE, crane cert, operator license, crane capacity In the field

Ju will perform the following inspection items:

- Bearing pad elevation
- Bearing pad
- Girder (length, release tag, cert., dimensions)
- Cambers
- Temporary cross-bracing
- Retaining Walls

RETAINING WALL 1 AND 2: Retaining Wall 1 and 2 appear to be Type 5 (Case1) CIP Wall and masonry wall, respectively. For the footing of the CIP portion, Ju will verify the layout, elevation and dimensions of the footing. Prior to the concrete pour of the footing, he will verify that the contractor places the rebar for both the footing and the wall stem at the right location. This will ensure that the retaining wall layout line is placed correctly for the stem pour. After the footing pour, our team surveys and verifies the top of the CIP wall. In addition, Ju will also “eye ball” the top of wall chamber on the stem forms to ensure that the top of wall is smooth and transitions well along the length of the wall. He will make sure the contractor places the wall expansions, weep holes and weakened plane joint correctly per the Caltrans Standard Plans and the grading plans.

POTHOLING: Potholing of existing utilities shall be utilized prior to all aspects of work involving excavation, including drilling for the CIDH piles, removals of pavement, curb and gutter, base materials, sidewalks, retaining walls, fencing, streetlights, trees, and other landscaping; relocation of landscape features, existing fencing, gas line markers, and electrical and irrigation boxes. We will ensure the contractor call in a Dig Alert prior to any excavation activity.

UTILITY COORDINATION

The project relocates current utilities and provides for future utilities to be supported by the new Sepulveda Bridge over the channel. Our team will ensure the contractor incorporates the allowable work windows in the baseline schedule and assists the City with planning and coordination of work with SCE and other utility companies. We take a proactive approach by using a Utility Matrix and reviewing contract documents and available utility as-built plans in order to evaluate potential conflicts. Any potential conflicts or issues identified in the Utility Matrix are presented to the City Representative and discussed during pre-construction meetings and at the weekly meetings. Where potential conflicts are identified, special workshops are scheduled with the CM team, prime contractor, subcontractors, and utility owners to address issues and concerns in order to avoid potential schedule delays.

Our team uses a proactive approach on all aspects of construction, and that extends to utilities. This project has a variety of utilities with differing needs throughout; existing utilities, those to be protected in place and/or to be relocated by others and those to be constructed as contract work. These all have possible impacts to the construction project. To avoid conflicts, potential delays and additional costs, lines of communication with representatives of each affected utility will be developed well before start of construction.

The project Utility Plans show an extensive presence of existing utilities within the project limits, with most being shown as not in conflict with the planned project improvements. However, on both the westbound and eastbound sides of the project, Southern California Edison has existing poles to be relocated with the relocations to be addressed by Edison crews prior to start of construction. With the major component of the project being the widening of the bridge over the Dominguez Channel, timing of the Edison relocation work is critical to the schedule of the project. Coordination with Edison will be one of the first priorities of our team at the onset of the project and may play an important element in the contractor's CPM schedule.

The tables below outline the Utility Matrix that the EXP team has begun to develop for this project.

Owner	Plan Sheet No.	Location	Utility Description	Pre-Activity Mtg Date	"E = Est. Start A = Act. Start"	"E = Est. Finish A = Act. Finish"	Latest Date of Approved Plans	Conflict Location	Utility Conflict/ Work Description	Depth (ft)	"Util. Reloc. A = Abandon RB = Replace Before RD = Relocate During PP = Protect in Place NC = No Conflict"	"Resp. Party U = Utility Co. C = Contractor"	Required Completion Date	Comments
California Water Service	C-5	41.5' LT 14+07	Fire Hydrant	TBD				41.5' LT 14+07	Conflicts with Roadway Widening	Surface	RD	U	During Construction	Coordinate with owner during Construction
California Water Service	C-5	41.7' LT 16+47.6	Water Meter	TBD				41.7' LT 16+47.6	Conflicts with Roadway Widening	Surface	RD	U	During Construction	Coordinate with owner during Construction
California Water Service	C-5	41' LT 17+42	Fire Hydrant	TBD				41' LT 17+42	Conflicts with Roadway Widening	Surface	RD	U	During Construction	Coordinate with owner during Construction
California Water Service	C-6	29' LT 23+22	Fire Hydrant	TBD				29' LT 23+22	Conflicts with Roadway Widening	Surface	RD	U	During Construction	Coordinate with owner during Construction
California Water Service	C-7	37.3' LT 25+67.6	Fire Hydrant	TBD				37.3' LT 25+67.6	Conflicts with Roadway Widening	Surface	RD	U	During Construction	Coordinate with owner during Construction
SCE	C-4	55' LT 10+53	Power Pole					n/a			PP	C		No action required
SCE	C-4	44.6' LT 10+65.8	Power Pole					n/a			PP	C		No action required
SCE	C-4	41.5' LT 12+37	Power Pole	TBD				41.5' LT 12+37	Conflicts with Roadway Widening	Surface	RB	U	Prior to Construction	Coordinate with SCE prior to Start of Construction
SCE	C-4	49.6' LT 12+48	Power Pole					n/a	n/a		PP			No action required
SCE	C-5	41.2' LT 14+01	Power Pole	TBD				41.2' LT 14+01	Conflicts with Roadway Widening	Surface	RB	U	Prior to Construction	Coordinate with SCE prior to Start of Construction
SCE	C-5	22.8' RT 14+16	Electrical Transmission Pole	TBD				22.8' RT 14+16	Conflicts with Roadway Widening	Surface	Remove	U	Prior to Construction	Coordinate with SCE prior to Start of Construction
SCE	C-5	43.8' RT 14+17	New Power Pole w/guy wires	TBD				43.8' RT 14+17			New Pole	U	During Construction	Coordinate with owner during Construction
SCE	C-5	41.7' LT 15+56	Power Pole w/guy wire	TBD				41.7' LT 15+56	Conflicts with Roadway Widening	Surface	RB	U		
SCE	C-5	23.3' RT 15+69	Transmission Pole w/guy wire	TBD				23.3' RT 15+69	Conflicts with Roadway Widening	Surface	Remove	U		
SCE	C-5	43.8' RT 15+68	Power Pole w/guy wire	TBD				43.8' RT 15+68			New Pole			

Owner	Plan Sheet No.	Location	Utility Description	Pre-Activity Mtg Date	"E = Est. Start A = Act. Start"	"E = Est. Finish A = Act. Finish"	Latest Date of Approved Plans	Conflict Location	Utility Conflict/ Work Description	Depth (ft)	"Util. Reloc. A = Abandon RB = Replace Before RD = Relocate During PP = Protect in Place NC = No Conflict"	"Resp. Party U = Utility Co. C = Contractor"	Required Completion Date	Comments
SCE	C-5	41.5' LT 17+34	Power Pole w/ guy wire	TBD				41.5' LT 17+34	Conflicts with Roadway Widening	Surface	RB	U	Prior to Construction	Coordinate with SCE prior to Start of Construction
SCE	C-5	41.3' RT 18+07.8	Transmission Pole w/guy wire	TBD				41.3' RT 18+07.8	Conflicts with Roadway Widening	Surface	RB	U	Prior to Construction	Coordinate with SCE prior to Start of Construction
SCE	C-6	40.7' LT 19+14	Power Pole w/ guy wire	TBD				40.7' LT 19+14	Conflicts with Roadway Widening	Surface	RB	U	Prior to Construction	Coordinate with SCE prior to Start of Construction
SCE	C-6	44.7' lt 19+99	Power Pole w/ guy wire	TBD				44.7' lt 19+99	Conflicts with Roadway Widening	Surface	RB	U	Prior to Construction	Coordinate with SCE prior to Start of Construction
SCE	C-6	42.3' RT 20+28	Power Pole w/ guy wire	TBD				42.3' RT 20+28	Conflicts with Roadway Widening	Surface	RB	U	Prior to Construction	Coordinate with SCE prior to Start of Construction
SCE	C-6	38.4' RT 23+45.3	Transmission Pole w/guy wire	TBD				38.4' RT 23+45.3	Conflicts with Roadway Widening	Surface	RB	U	Prior to Construction	Coordinate with SCE prior to Start of Construction
SCE	C-7	45.3' LT 25+07.6	Power Pole	n/a				n/a	n/a		PP	C		No action required
SCE	C-7	44.0' RT 25+24	Transmission Pole	n/a				n/a	n/a		PP	C		No action required
SCE	C-7	44.0' RT 28+63	Transmission Pole	n/a				n/a	n/a		PP	C		No action required
TBD	C-4	62' LT 10+00	"Nitro Vent"					n/a	n/a		PP	C		No action required
TBD	C-4	50.7' LT 10+38	Riser					n/a	n/a		PP	C		No action required
TBD	C-4	42.6' LT 12+78	Existing electrical pull box					42.6' LT 12+78	Conflicts with Roadway Widening	Surface	RD	C	During Construction	Coordinate with owner during Construction
TBD	C-4	41' RT 13+10	Utility Risers (3)	TBD				41' RT 13+10	Conflicts with Roadway Widening	Surface	RD	U	During Construction	Coordinate with owner during Construction
TBD	C-4	35' RT 13+11 to 28.7' RT 13+30	Utility Pipes (2)	TBD				35' RT 13+11 to 28.7' RT 13+30	Conflicts with Roadway Widening	TBD	RD	U	During Construction	Coordinate with owner during Construction
TBD	C-4	31.5' RT 13+67	Utility Vault	TBD				31.5' RT 13+67	Conflicts with Roadway Widening	TBD	RD	U	During Construction	Coordinate with owner during Construction
TBD	C-5	41.3 lt 17+58.5	Utility Vault	TBD				41.3 lt 17+58.5	Conflicts with Roadway Widening	Surface	RD	U	During Construction	Coordinate with owner during Construction
TBD	C-6	31.2' LT 23+34.6	Utility Vault	TBD				31.2' LT 23+34.6	Conflicts with Roadway Widening	TBD	RD	U	During Construction	Coordinate with owner during Construction
TBD	C-6	31.0' LT 23+42.2	Utility Vault	TBD				31.0' LT 23+42.2	Conflicts with Roadway Widening	TBD	RD	U	During Construction	Coordinate with owner during Construction
TBD	C-7	41.3' RT 25+80	Electrical Pull Box	TBD				41.3' RT 25+80	Conflicts with Roadway Widening	Surface	RD	C	During Construction	Coordinate with owner during Construction
TBD	C-7	44.0' RT 26+51	Fire Hydrant	n/a				n/a	n/a		PP	C		No action required



Traffic Management

Traffic control plans, prepared by a registered Civil or Traffic Engineer, shall be provided by the contractor allowing for approval 10 days prior to the start of construction. The contractor shall also apply for a no-fee traffic control permit from the City's Engineering Division for any type of street closure, full or partial. The contractor shall provide the contact information for the person responsible for maintenance of traffic control devices and other safety items related to traffic control.

Traffic control shall be in accordance with the Work Area Traffic Control Handbook (WATCH) prepared by the Southern California Chapter of the American Public Works Association. All traffic control work shall be performed by a contractor possessing a Class "C31" license.

No street within the City of Carson shall be closed without approval of the RE in conjunction with the City's Project Manager. Any approved plan involving a street closure shall be submitted with sufficient advance of any rerouting or diversion of traffic for review and approval. Plans shall also address how access to each adjoining property shall be maintained during construction.

Where less than 2 lanes in each direction are open to traffic, the contractor's working hours are limited to 8:30 a.m. through 3:30 p.m. work hours are 7:30 a.m. to 4:00 p.m. when 2 lanes are open in each direction. Traffic control devices shall conform to the current State of California, Department of Transportation, "Manual of Warning Signs, Lights, and Devices for Use in Performance of Work upon Highways". Two-way traffic shall be always maintained through the project.

Traffic Signals

Traffic signal modifications will be required at the Alameda Street intersection. The EXP team has administered many traffic signal improvements throughout Southern California including San Marcos, Camarillo and Bakersfield. We are well versed in reviewing submittals such as CCTV cameras, fiber optic cable, pull boxes conduit, schedule of values, wire, signal standards prior to fabrication and verifying that all the welding meets the contract plans and specification. In addition, we will verify proper notice of material forms have been submitted with all noted material to be used. During the course of the project, Ju monitors new and modified electrical installations performed by the Contractor.

Pedestrian Access

The contractor's Traffic Control Plans shall also address pedestrian traffic through and around the work area.

The contractor is responsible for accommodating pedestrians through the work zone whenever the work disrupts existing pedestrian access. If the contractor's activity requires closing a pedestrian pathway, another pathway must be made available. The contractor's work activities shall not impede the flow of pedestrian traffic through the project at any time. A barricaded area in a parking lane shall be provided during times where a parkway is unfit for pedestrian traffic. Accessible pedestrian routes must be maintained throughout the work area. If a safe alternate route is not available without crossing the roadway at an unsignalized intersection, then a temporary route shall be created with separation from both vehicular traffic and the work site.

When planning the temporary facility, the following items should be considered:

- A pedestrian route must not be severed or moved for non-construction activities such as parking for vehicles and equipment
- Barriers, detectable by a person with a visual disability traveling with the aid of a long cane, must be placed across the full width of the closed sidewalk they would normally use
- A reasonably safe route that does not involve crossing the roadway must be provided. If this is not possible, advance signing should direct pedestrians to cross to the opposite side of the roadway
- Midblock work sites should not induce pedestrians to attempt skirting the work site or make a midblock crossing
- Pedestrian movements should be separated from both work site activity and vehicular traffic. When pedestrians are routed adjacent to live traffic, barrier protection must be provided to prevent vehicles from entering the pedestrian facility
- Tape, rope, or plastic chain strung between devices as controls for pedestrian movements must not be used. They are not readily detectable by persons with a visual disability
- Where barricades channel pedestrians, continuous detectable bottom and top rails must be used, with no gaps between individual barricades for users of long canes. The bottom of the bottom rail must be no higher than 2 inches above the ground surface. The top of the top rail must be at least 32 inches above the ground
- During the inspection process, it is recommended that checking of slopes should utilize a 2-foot-long smart level on elements with a dimension of 4 feet or less, and a smart level at least 4 feet long on elements with a dimension greater than 4 feet to verify compliance with ADA requirements.

Our team has visited the site and studied the plans to identify a method to keep the pedestrian access and still construct the WB and EB widenings of the bridge. This is discussed and shown in detail in the Technical and Procedural Innovation section on page 24.

TEMPORARY CONSTRUCTION EASEMENT

After our review of the Plans and Specifications provided for this RFP, there does not appear to be any TCE requirements at this time. However, per the mandatory pre-proposal/job walk, it was stated that the CM consultant will be assisting the City with TCE's. Our team has successfully managed TCE's on all of our projects, including the **OCTA I-405 Improvements Project**, with a TCE matrix. The matrix tracks TCE dates, durations, and requirements per the agreements between the City and the property owners. The TCE matrix is a living document and will be placed in a cloud base platform so the City and our team have access at all times. It identifies any critical dates and any details that Anh and her team need to keep abreast of to ensure the Contractor adheres to when he enters the TCE. With the use of the TCE matrix and constant coordination with the Contractor, we will ensure that all the property owners and the business in the area are minimally impacted.

OUTLINE OF ACTIVITIES

The EXP Team has extensive experience in delivering projects similar to the Sepulveda Widening Project from pre-construction to project delivery. We pride ourselves in being at our client's side every step of the way to hand over a turn-key project. The Outline of Activities table below identifies each activity throughout the life of the project and who is responsible.

LEGEND	Anh Case, PE - PM / RE / SR	Ju Kim, PE, QSD - Sr. Inspector	Tim Kirkley, LSIT - OE/Utility Coordinator	David Hamilton, PE, GE, H+A - Materials Testing	Arvin K. Chaudhary, PE, PLS - Surveyor	Kim Morales - Labor Compliance	Eric Turner - Environmental Service
Pre-Construction							
Constructability & Plan Review/Analysis	P	P	P	S	S		S
Bid Support	P	S	S				
Advance Utility Coordination/ Early Construction Activities	P	S	P		S		C
Value Engineering	P	S	S				
Set Up Construction Project Files	P		P				
Initial Construction Schedule Review	R	S	S				
SWPPP Review	R	S	S				C
Monthly Reporting and Invoicing	P						
Pre-Construction Photo Documentation	P	P	P				
Construction Staking Package and Control	S	S	S		P		
Pre-Construction Meeting	P	S	P	S	S	S	S
Construction Management							
Coordination (City, Utilities and Other Stakeholders)	P	S	P				C
Conferences/Meetings	P	P	P	S		S	S
Pre-Activity Meetings	P	S	S	C	C		C
Weekly Statement Of Working Days	P						
Shop Plans Review	R	S	S				
Construction Work Plan Review	R	S	S	C	C		C
Address Requests for Information (RFIs)	P	S	S	C	C		C
Contract Change Order's (CCO)	P	S	S				
Analysis of Notices of Potential Claims	P	S	S				
Risk Management + Risk Register	P	S	S				C
Progress Pay Estimates	P	S	P				
Schedule Review and Update	R	S	S				
Contract Compliance	P	P	P	P	P	P	P
Inspection of Contractor's Work for Compliance	P	P	P	P	S	P	P
Daily Report of Contract Operations	P	P	P	P			S
Materials Sampling and Testing for Quality Assurance	P	S	S	P			
Plant Inspection for Quality Assurance	S	S		P			

Legend	Anh Case, PE - PM / RE / SR	Ju Kim, PE, QSD - Sr. Inspector	Tim Kirkley, LSIT - OE/Utility Coordinator	David Hamilton, PE, GE, H+A - Materials Testing	Arvin K. Chaudhary, PE, PLS - Surveyor	Kim Morales - Labor Compliance	Eric Turner - Environmental Service
P – Primary Responsibility S – Support Role R – Review/Approve C – Consult if Necessary							
Construction Management - <i>continued</i>							
Source Inspection	S	S		P			
TMP Implementation During Construction	P	P	S				
Administer DBE Requirements, Labor Compliance and Prevailing Wage	S	S	P			S	
Plant Inspection for Quality Assurance	S	S		P			
Source Inspection	S	S		P			
TMP Implementation During Construction	P	P	S				
Administer DBE Requirements, Labor Compliance and Prevailing Wage	S	S	P			S	
Certified Payroll	S	S	P			S	
Contractor's Operations Relative to Safety Requirements	P	P	P				S
Safety and Maintenance Reviews	P	P	P				
Public Outreach	P	S	P				S
Log of Business Owner's Concerns and Inquiries	P	S	P				
TCE Management & Coordination	P	S	P		C		
Attend City Council Meetings	P	S	S				C
Coordination of Partnering Workshops	P	S	S				S
Updated Environmental Commitments Record	S	S	S				P
Resource Agency Permit Renewal and Extension Requests	P	S	S				P
Post - Construction / Close Out							
Punchlist	P	P	P				
Final Inspection and Acceptance Recommendation	P	P	P	C			
Proposed Final Contract Estimate	P	S	P	C			
As-Built Plans	P	S	P	C	C		C
Final Report	P		P				
Certificate of Environmental Compliance	S		S				P
Contract Acceptance	P		P				

TECHNICAL AND PROCEDURAL INNOVATIONS

TECHNICAL INNOVATIONS

Today, owners require comprehensive project documentation and risk avoidance plans from their partners. EXP is excited about the tools available today that seek to bring our projects successfully into the future by implementing software such as BlueBeam, OpenSpace, Headlight, and Procore. We have and continue to work with different technologies that best fit our projects and have identified these to bring value to this project.



BlueBeam allows you to simplify complex processes and collaborate in real-time with team members from wherever they are located. BlueBeam will keep the team on the same page through the design process, helps move the project forward during construction, and preserves important project data through completion and beyond. This software has been vital on our current projects. Real-time interaction with the designer and the City of Carson's staff will allow efficiencies in the following: Design review, QA/QC, takeoffs, document management, submittal review, and RFI postings. We are currently using Bluebeam on the Hesperia Ranchero Improvement Project to increase the efficiency and effectiveness of collaboration of all stakeholders.



OpenSpace is a document control tool that captures 360-degree images which are then stitched to site plans providing split-screen images of current days activities and any other past day activities in the same exact area. Ju and Anh used OpenSpace on the OCTA I-405 Design-Build Project and it has been proven invaluable in pre-construction services, claims avoidance, or dispute resolution, and is a historical record, which provides information for as-built design's in the future if needed.



HeadLight project management software enables our team to capture activities in the field in real-time – with photo-driven reporting that provides far more detailed information than words alone. Instead of pen-and-paper logs, teams can make project updates, record issues, and generate client reports on an iPad – right from the job site. The system updates automatically so managers, inspectors, and owners alike can

see exactly what's happening at the moment it happens. This results in:

- Faster Reports
- Increased On-Site Inspection
- Live Project Feed
- Streamline Collaboration

Headlight is currently being used in Caltrans District 8 where EXP is the Project Management Consultant.

The EXP Team had used Procore Construction Management on the IVDA Mtn. View Bridge Project which incorporated the latest tablet and cloud-based technology. This technology allows our inspectors to be more efficient, allowing more time for actual field inspection and allowing everyone real-time access to document files or issues that may arise at the field level.

EXP's vision for the processes, systems, and tools is to standardize on a platform that is reliable, expandable, repeatable, and integrated with other industry-standard technology. To meet this need, at EXP we use Procore software to connect everyone and everything during the lifecycle of the project (from bidding to closeout) on one platform that is easily accessible by the project team. Procore is a secure and trusted multi-zone cloud infrastructure tool that runs the project with complete visibility. Since the tool includes many modules and allows easy integration with other tools and systems (scheduling, accounting, BIM, document management, reporting) the communication is streamlined and increases the efficiency by:

- Creating a collaborative space
- Unified view of project performance and allow for data-driven decisions
- Flexible tool that offers the flexibility needed to create new processes and workflows

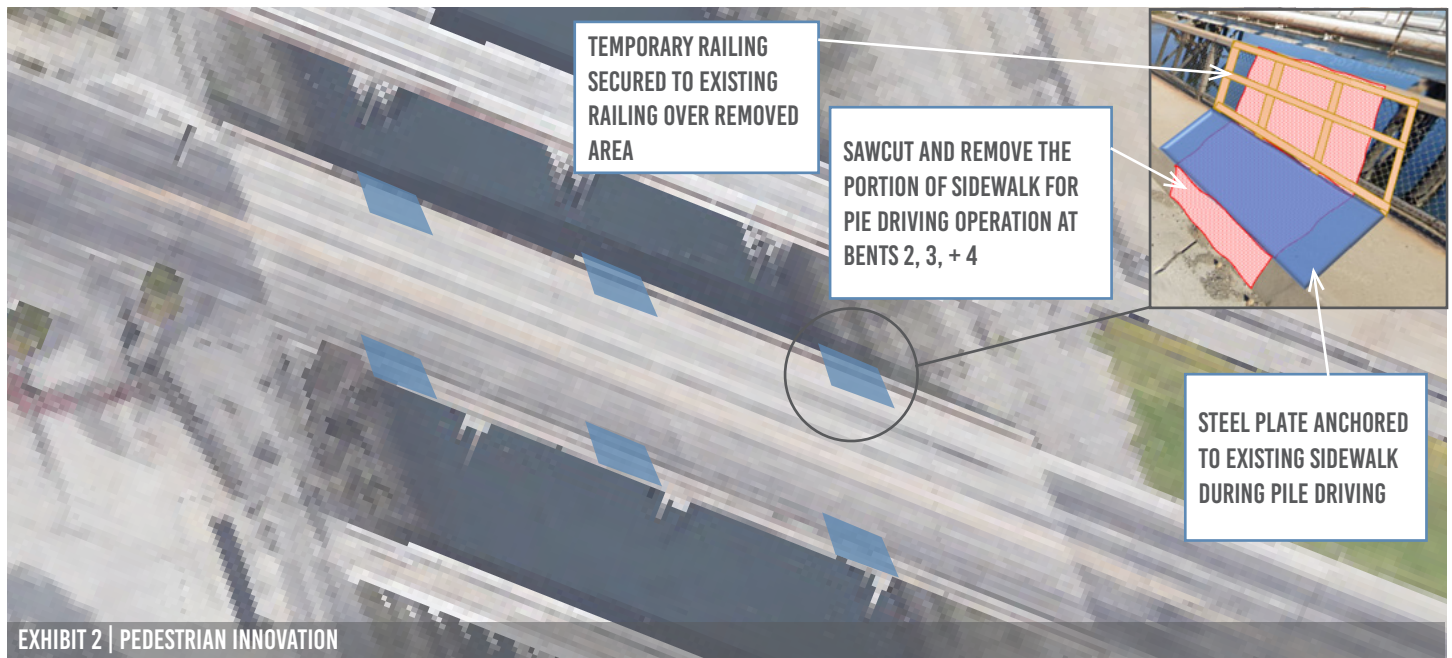
The EXP Procore configured and integrated system is built on three commitments:

- Technology - Data-Centric, Interoperable, Single Source of Truth, Cloud-First Platform
- Process – Optimization and efficient processes to improve workflow and communication
- People - Knowledge Management, Continuous Learning & Collaboration

The final Procore configuration and stack will meet the end goal to create a single source of information, to maximize interoperability, and serves the goal of creating a digital twin to plan, design, operate, and maintain the assets.

PEDESTRIAN ACCESS INNOVATION

During our site visits and review of the plans, we have developed an innovative way of keeping the pedestrian access open at all time during construction. The contractor can remove just the portion of the sidewalk and overhang affecting the pile operation. The advantage is that once the piles are driven, we can plate the gap over for pedestrians and proceed with the same method on the other side of the bridge. This option allows the contractor to keep the pedestrian walkway open. This will also help with one mobilization for the piling rig and speed up the installation of the piles for the bents. This innovation is shown in the exhibit below.



QUALITY CONTROL

Anh will establish a project-specific Quality Assurance Plan based on the contract documents for inspectors, material control, and environmental requirements. Inspections are conducted in coordination with the contractor in a timely manner to avoid delays to the project schedule and rework. Ju and Tim will record daily activities or non-activities in Daily Records, which include quantities of work performed, progress photographs, and relevant verbal correspondence. Anh will review all Daily Records for accuracy and consistency and provide them to the City Representative. We will check all of the contractor layouts and control points. Prior to the start of construction, we will identify and establish a log of materials to be used per the contract specifications. We will coordinate with the contractor to provide the list of materials and their respective source that are incorporated into the work. We will identify materials that require sampling, including source inspection, and testing and materials that can be released on-site for use. We will review the contractor's Welding Quality Control Plan (WCQP), shop and field, Pre-cast Quality Control Plan (PCQP), Pile Installation Plan, CIDH Installation Plan, and shop drawings per the contract documents. We will verify materials being used are per the approved submittals and inspected and released by our source inspector, Hamilton & Associates. In the event of material, equipment, workmanship, or construction discrepancies, Anh will inform the City Representative, and the contractor is notified with a non-conformance report and request for corrective action.

BUDGET CONTROL

We understand the importance of delivering projects within budget. We have a history of completing construction projects on time and within budget. Value Engineering is an excellent method to reduce project costs.

We are constantly looking for means and methods to reduce the project cost while being thoughtful to not

create impacts on the public or the cities. Our team is experienced in identifying and quantifying items to enhance the quality of the project while reducing lifecycle costs. Through partnering, the contractor is encouraged to also find ways and means that benefit both the contractor and the City, providing a win-win situation. Using experience, the CPM schedule, and a five-week look-ahead schedule, we are proactive in “looking ahead” of the contractor’s operations to identify issues that may impact costs, and we make every effort to provide solutions and present them to the City Representative. If an extra cost item cannot be completely eliminated, we evaluate the contractor’s change order requests to ascertain validity, merit, and appropriate costs based on an independent cost estimate. Contractor monthly payment estimates are reviewed for content and financial accuracy and are certified by Anh. We will provide the City Representative with a monthly status report of contractor and consultant costs, including costs to date compared to the contract time expended, remaining contract time and cost, potential, and actual change orders, and a narrative evaluating the project cost status.

CONTRACT PROGRESS PAYMENTS

We will review and recommend payment for the contractor’s monthly progress payment requests and maintain a schedule of earnings throughout the contract. Every month, Ju prepares quantity calculations for each bid item. We also monitor CCO works and verify quantities completed each month. We will meet with the contractor and review each bid item and CCO amount to verify correct pay amounts and provide appropriate backup documentation for each item. We review the contractor’s certified payrolls, DBE reports, and EEO records to verify labor compliance. We will review progress reports, lien waivers, and inventories of materials on hand; quantity surveys; and monthly schedule updates. We also verify that retention and other applicable deductions are properly recorded on each progress pay estimate.

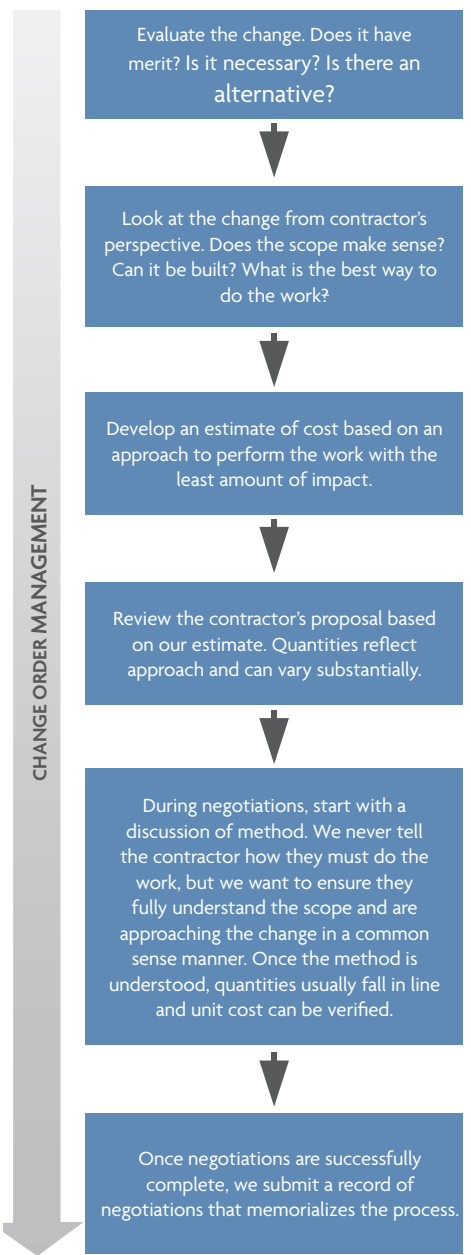
CHANGE ORDER MANAGEMENT

Exhibit 3 depicts our change order management process. If a potential extra cost item is identified and cannot be completely avoided, then we identify and monitor the issue in question. We use procedures to efficiently collect information and document the complete impact on the project. We track any changes to the construction contract initiated by the City in the same manner as those initiated by the contractor. We request cost proposals from the contractor and ascertain validity, merit, and appropriate costs. Concurrently, Anh performs an independent cost analysis or negotiates Time and Materials costs with the contractor. Once an agreement has been reached with the contractor, she prepares a recommendation for you containing a detailed description of the item, justification for the recommendation, and backup information. Upon agreement, Anh prepares an acknowledgment letter to the contractor for you with the terms of the change. Anh assists you with the preparation and submittal of a contract change order per the City’s format for written approval. No work is allowed prior to the approval of an executed change order. Ju documents all work related to CCOs in his Daily Record. For extra work approved on a time and material basis, Ju prepares a daily extra work report per the Caltrans Construction Manual.

CLAIMS MANAGEMENT

The first step in effective claims management is to be proactive in “looking ahead” of the contractor’s operations to identify issues that may impact cost and make every effort to provide solutions before the issues become cost issues.

EXHIBIT 3 | CHANGE ORDER MANAGEMENT



RISK REGISTER

Prior to the start of the project, we develop a Risk Register similar to that shown below. It identifies all risks associated with the project and the measures taken to lower or mitigate the impact to the project. We constantly look ahead to identify and resolve issues before they can develop into a claim. When we become aware of a claim either by verbal or written communication, we immediately notify the City Project Manager. We will provide you with a detailed explanation of the claim and propose resolution alternatives with a recommendation as to the preferred solution. We also support you in resolving claims and disputes by providing written responses for the contractor and private parties, investigation of claims for damages by private parties, assistance with dispute resolution, arbitration, or litigation; providing expert witness; and services required due to contractor default. In addition, we assist you with negotiating claims to their agreed conclusion. Our Team has a thorough, in-depth understanding of the risks associated with working over water as well as challenges, bringing forth the experience and knowledge to implement processes and procedures to successfully deliver a high-quality product for the City of Carson. A detailed review of the risks related to the Sepulveda Bridge project can be found in the table below.

NO.	ISSUE	DESCRIPTION	SOLUTION	BENEFIT
1	Utility Relocations/Coordination	Delayed completion of advanced utility relocations (i.e., SCE) may impact the project schedule.	<ul style="list-style-type: none">Third Party engagement and routine coordinationHave plan for temporary relocation of overhead utilities, if applicableDevelop workaround to minimize impact the schedule	Maintain or shorten project schedule
2	Pedestrian Access	Contractor failing to provide adequate pedestrian access. Traveling public may complain to the City.	<ul style="list-style-type: none">Require Contractor provide a Temporary Pedestrian Access Route (TPAR) at the beginning of the project.Ensure the Contractor identifies the TPAR in the Baseline ScheduleWork closely with the Contractor and the City or City Consultant to approve the TPAR quickly to allow early implementation	Will provide for the safe passage through the work zone for all pedestrian traffic while meet MUTCD and ADA requirements
3	Long Lead Items: <ul style="list-style-type: none">Pre-cast Pre-stressed GirdersPre-cast PilesBearing Pads	Delayed procurement could impact the project schedule.	<ul style="list-style-type: none">Require contractor to provide accurate CPM Baseline schedule. Keep schedule updates timely.Review updated schedules and ensure contractor adheres to procurement schedulesContinuous follow ups regarding status of procurement and delivery of items	Maintain or shorten project schedule
4	Abandoned utilities still active / hazardous	Uncovering an abandoned fuel oil line or asbestos conduit will delay the contractor from continuing on contract work until remediation is complete. This takes time and usually little to no work around.	Require the Contractor to have a remediation company subcontractor on board from day one of the project and have them required to be “on-call” 24/7 in the event of a strike. This will negate any delays due to contractual on boarding and provide a swift remediation of the utility.	Maintain or shorten project schedule and provide safety for workers and the public
5	Utility protections and investigation	Damaging active utilities can impact the project schedule, incur additional cost and affect local businesses and residents. Safety can also be a concern.	<ul style="list-style-type: none">Ensure proper utility identification and protection plan is developed and adhered toImplement system for tracking utility locations.Place an encasement around sensitive utilities in coordination with utility owner.Thorough pre-bid utility surveyExtensive potholing program; Spill prevention plan; Emergency Response Plan for spill containment; Standby pumps and discharge containment vessels	Maintain or shorten project schedule and provide safety for workers and the public
6	Unidentified utility findings	Unknown utility findings may need to be removed, relocated or protected in place and can affect the schedule and/or the budget.	Initial field exploration including potholing and exploratory trenching and pre-trenching of proposed utility corridors	Maintain or shorten project schedule and provide safety for workers and the public
7	Contractor not adhering to Temporary Construction Easement (TCE) requirements	Contractor may have to stop work if TCE is expired and businesses may be upset.	<ul style="list-style-type: none">Develop a TCE matrix to track TCE durations and requirementsConsistent coordination with contractor and local businesses and residences	<ul style="list-style-type: none">Expired TCE would delay some aspects of the work and could delay completion of the projectMatrix would help RE keep on top of TCE's prior to expiration
8	Delivery and storage logistics of construction materials	Delivery and storage of materials can impact the quality of the product and the schedule of the project. For example, if the contractor chooses to store the light poles on site, damage and corrosion may occur during the storage duration.	<ul style="list-style-type: none">Evaluate adequacy of lay down areas; ensure contractor plans for haul routes, efficient use of Construction Staging AreaReview contractor's projected need for materialsMonitor supply chain through integrated on-line logisticsContinuous inspection of materialsHave contingency plan if damage occurs during delivery or storage	Maintain or shorten project schedule and minimize chance of contractor claims.
9	CIDH Pile anomaly	CIDH having anomaly	Pre-approved mitigation plan for minor repair	Having pre approved method for minor repair can allow the Contractor to proceed with repair without delaying the project schedule
10	Pile Driving - soft/hard driving	Encountering soft or hard driving condition during pile operation	<ul style="list-style-type: none">For soft driving, re-tap the piles after 24 hours.For hard driving, use driving tip.	Keep project on schedule
11	Spill in the channel	Equipment fluid or other spill in the channel	<ul style="list-style-type: none">Have a pre-approved plan and have a spill kit at the job site.Personnel trained to handle issues.	Minimize impact to the channel and preventing larger issue with Army Corp and County of Los Angeles Flood Control
12	Working in channel	Working in channel	Have life vest and flotation device on site	Safety of workers



SCHEDULE CONTROL

Within 15 days of Notice of Award, Anh will conduct a pre-construction scheduling meeting with the contractor, the contractor’s scheduler, and the EXP Team. Upon receipt of the contractor’s baseline schedule, Anh will review the schedule for compliance with the contract documents. We present our findings to the City Representative for input. Once the input has been obtained, the schedule undergoes a final review for "fatal flaws". Possible corrections and enhancement recommendations are returned to the contractor for inclusion of comments. For the project to move forward in a timely manner, this process should take no more than two weeks.

The application of a partnering concept provides a powerful method of communication that fosters an atmosphere of trust and cooperation. Throughout the duration of the project, we hold meaningful regular partnering sessions with the stakeholders to maintain free-flowing and productive lines of communication that drive the schedule. Each month, prior to the submittal of the updated schedule, Anh sits with the contractor to “pencil in” the progress of the project. This alleviates any back-and-forth on the review of the schedule. Once agreed upon, the contractor submits an updated project CPM schedule. The updated schedule is compared to the baseline schedule, and any significant changes are addressed by our staff and communicated to the City Representative along with any impacted stakeholders. Swift action regarding schedule changes is required to deliver the project on time and reduce impacts to the public. We recommend at each weekly progress meeting, the contractor submits a five-week schedule outlining the specific tasks to be undertaken. We and other stakeholders use this information for workforce scheduling and conflict avoidance. The most important method used to deliver a safe, quality project is the practical application of the personnel training and knowledge of the members of our team. We provide you and all the stakeholders with a strong, experienced, “look ahead”- driven extension of staff at the project site.

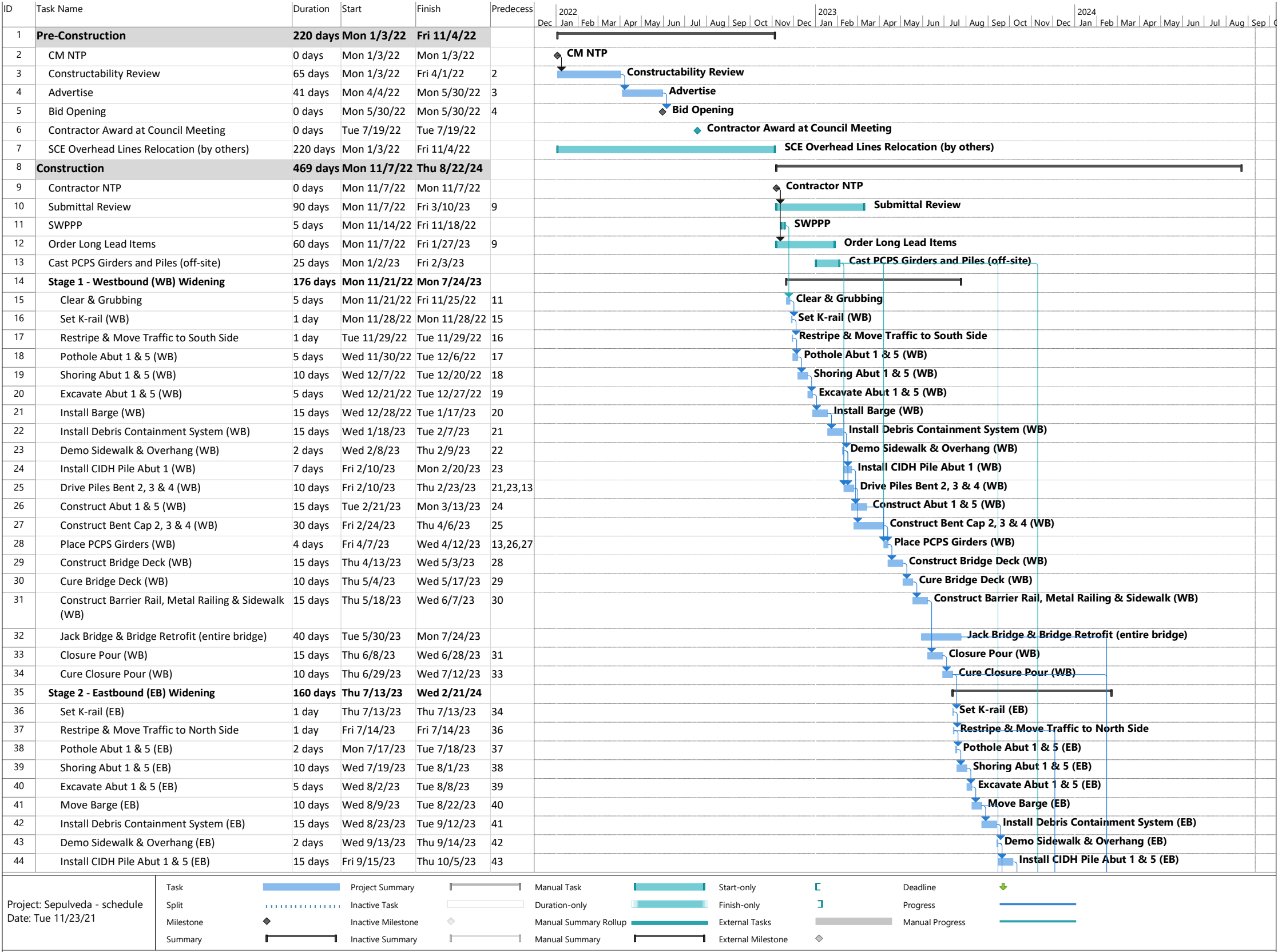
PROJECT SCHEDULE

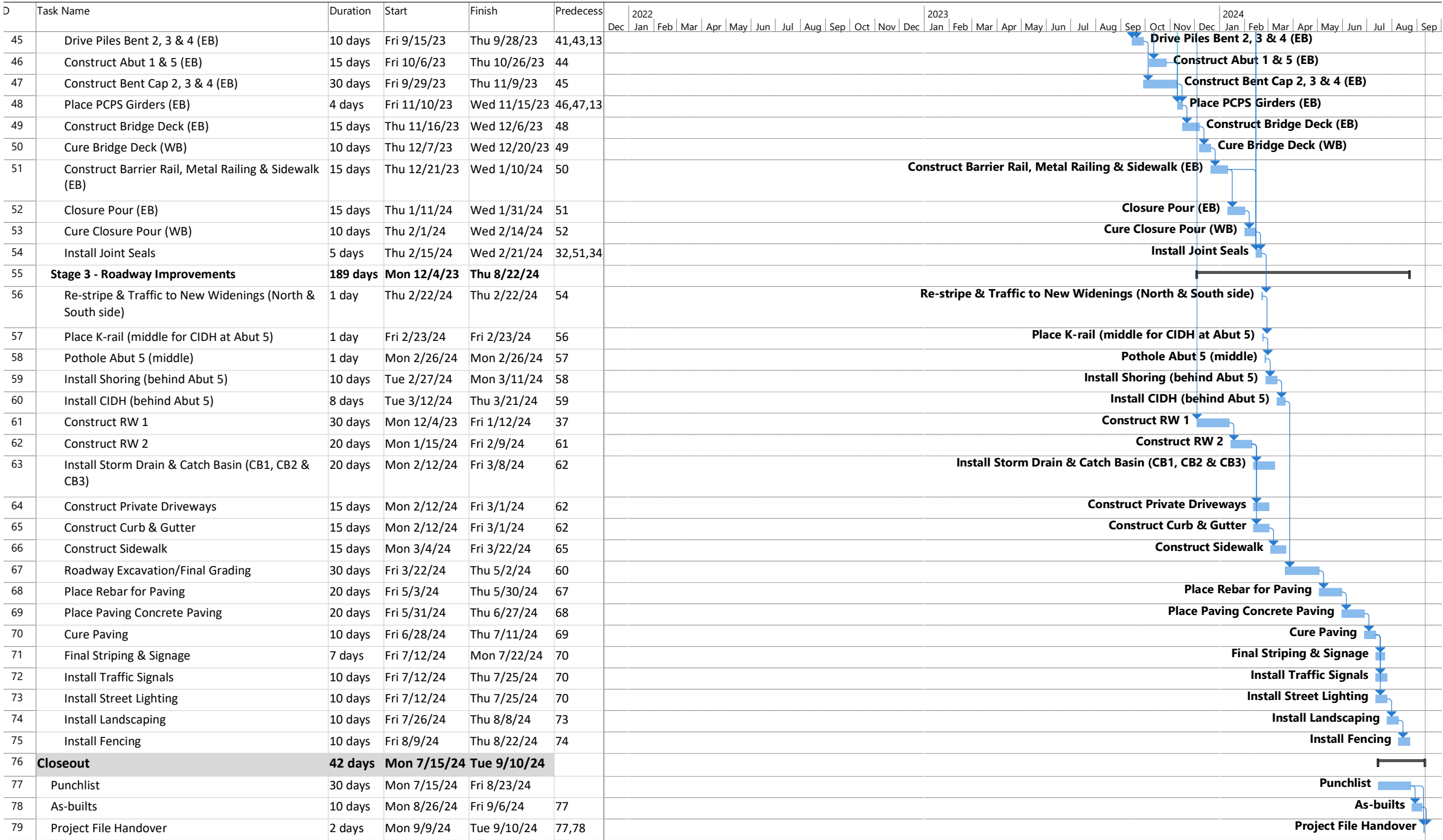
The following project schedule is based on an estimated construction contract time described in the RFP and per our Approach #1 as described in our Work Plan.

We understand that the RFP states construction is anticipated to last for 24 months. However, our team has reviewed the Plans and Specifications and we believe the project can be built in 20 months per the schedule (to the right and next page). The baseline schedule is based on the three construction stages with the associated milestones after the award of the contract and contractor mobilization.



THIS RESULTS IN A SAVINGS OF 4 MONTHS TO THE SCHEDULE AND 4 MONTHS OF CONSTRUCTION CAPITAL AND CONSTRUCTION MANAGEMENT COSTS.





Project: Sepulveda - schedule
Date: Tue 11/23/21

Task

Split

Milestone

Summary

◆

Project Summary

Inactive Task

Inactive Milestone

Inactive Summary

◆

Manual Task

Duration-only

Manual Summary Rollup

Manual Summary

Start-only

Finish-only

External Tasks

External Milestone

Deadline

Progress

Manual Progress





PROJECT TEAM

4 PROJECT TEAM

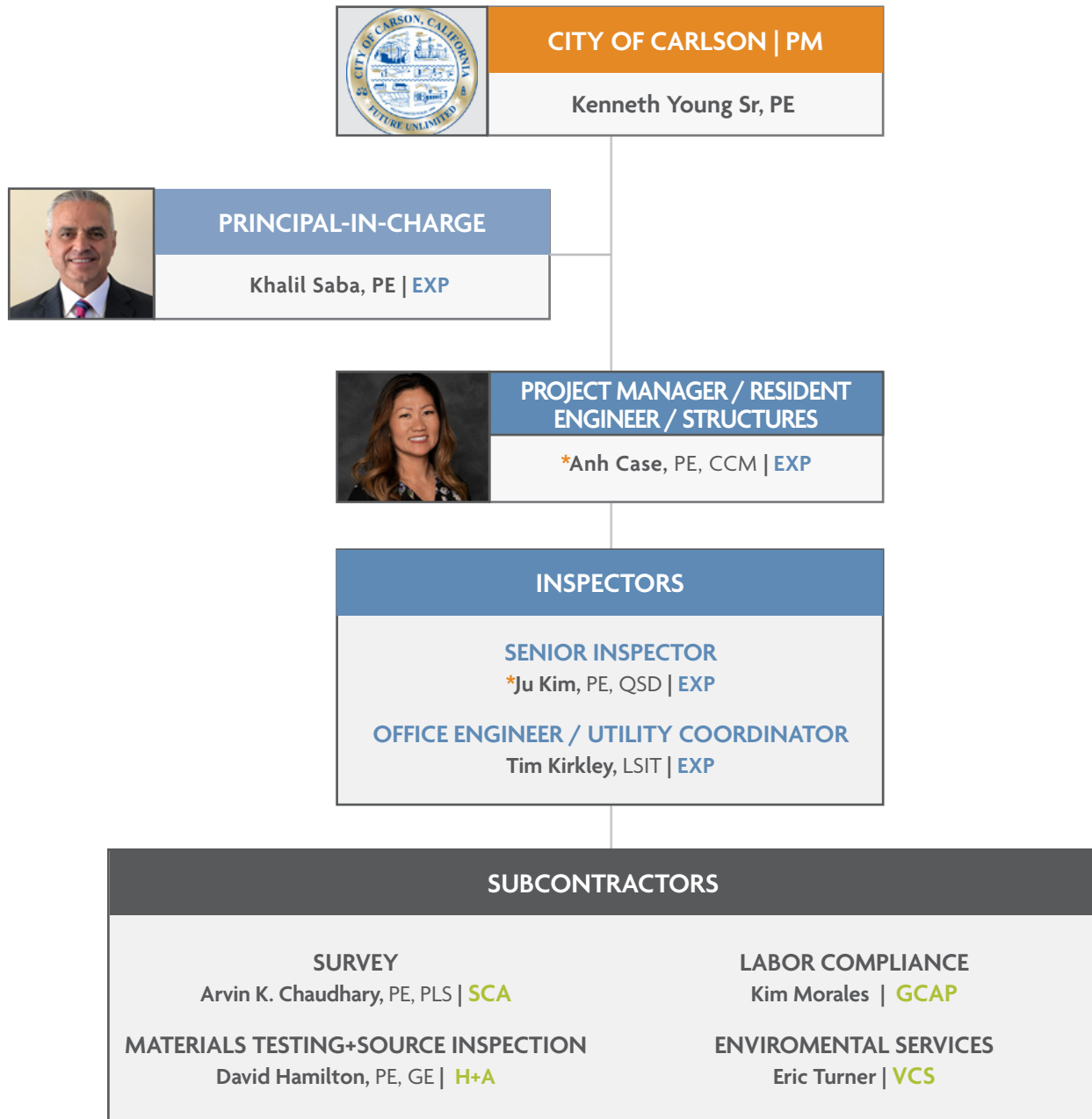
PROJECT TEAM MEMBERS

Anh Case, PE selected a diverse team of highly qualified professionals that have many skillsets and credentials that are showcased in the table below. For further details on their qualifications, please refer to the team resumes following the organizational chart.

PERSONNEL NAME + ROLE	EDUCATION + TRAINING	YEARS OF EXPERIENCE	REFERENCES
ANH CASE, PE EXP PROJECT MANAGER/RESIDENT ENGINEER/STRUCTURES REPRESENTATIVE	<ul style="list-style-type: none"> MS, Civil Engineering, University of California, Irvine BS, Civil Engineering, California Polytechnic State University, San Luis Obispo 	23+	Ryan Bishop Construction Manager (Retired) City of San Marcos t: 57+315.898.5385 (outside of US) e: rbishop521968@outlook.com Karen Cohoe, PE, PMP Project Manager Advanced Civil Technologies (ACT)/Inland Valley Development Agency (IVDA) 6 Hutton Centre Drive, Suite 450 Santa Ana, CA 92707 t: +1.714.253.3645
JU KIM, PE EXP SENIOR INSPECTOR	<ul style="list-style-type: none"> BS, Civil Engineering, University of California, Davis 	31+	Derek Chung Senior Bridge Engineer Caltrans District 7 601 S Glenoaks Blvd., Burbank, CA 91502 t: +1.626.202.7345 Phil Balmeo Senior Project Manager Moffit and Nichols – former SGVOG 4225 E Conant St., Long Beach, CA 90808 t: +1.562.708.0372
TIM KIRKLEY, LSIT EXP OFFICE ENGINEER UTILITY COORDINATOR	<ul style="list-style-type: none"> Various Mathematic and Economic Classes, San Bernardino Valley College 	41+	Garry Cohoe Senior Vice President Advanced Civil Technologies 3237 Guasti Road, Suite 230, Ontario, CA 91764 t: +1.909.563.8104 Bryce Johnston, Capital Projects Manager Riverside County Transportation Commission 4080 Lemon Street, 3rd Floor Riverside, CA 92501 t: +1.951.787.7141

PERSONNEL NAME + ROLE	EDUCATION + TRAINING	YEARS OF EXPERIENCE	REFERENCES
ARVIN K. CHAUDHARY, PE, PLS SCA SURVEYOR	<ul style="list-style-type: none"> BS, Aerospace Engineering, Boston University 	33	Brad Shumate, PLS Survey Branch Chief Caltrans District 4 111 Grand Avenue, Oakland CA 94612 t: +1.925.957.2141 Michael Spillane Assistant PM Webcor 175 Beale St., San Francisco, CA 94105 t: +1.510.205.6381
DAVID HAMILTON, PE, GE H+A PRINCIPAL GEOTECHNICAL ENGINEER	<ul style="list-style-type: none"> BS, Civil Engineering, Loyola Marymount University MS, Geotechnical Engineering, University of California at Berkeley 	22	Michael Tatalovich LACSD 1955 Workman Mill Rd, Whittier, Ca 90601 t: +1.562.699.7411 Philip Kang LACSD 1955 Workman Mill Rd, Whittier, Ca 90601 t: +1.562.699.7411
KIM MORALES GCAP SENIOR LABOR COMPLIANCE ANALYST	<ul style="list-style-type: none"> OCEMS EMT Accreditation, EMT Certificate, Santiago Canyon College Classes 	8	Crandal Jue Controller Metro Gold Line Foothill Extension Construction Authority 406 E. Huntington Drive, Suite 202 Monrovia, CA 91016 t: +1.626.305.7010 Paula Jurado Senior Labor Wage & Retention Programs Officer, Diversity & Economic Opportunity Department Los Angeles County Metropolitan Transportation Authority One Gateway Plaza Los Angeles, CA 90012-2952 t: +1.213.922.7669
ERIC TURNER VCS BIOLOGIST	<ul style="list-style-type: none"> MS, Urban & Regional Planning, emphasis in Environmental Planning & Policy, Cal Poly Pomona BA, History, University of California, San Diego 	11+	Paul Melocoton San Bernardino County Transportation Authority 1170 West 3rd Street, 2nd Floor San Bernardino, CA 92410 t: +1.909.889.8611 Jon Duston Knowleton Communities 3151 Airway Avenue, Unit B-3 Costa Mesa, CA 92626 t: +1.562.439.0281 x202

ORGANIZATION CHART



LEGEND

EXP	EXP	GCAP	GCAP Services (DBE)	VCS	VCS Environmental (DBE)
CA	Chaudhary + Associates (DBE)	H+A	Hamilton + Associates		

Anh Case, PE, CCM

Proposed Role: Project Manager | Resident Engineer | Structures Representative



SPECIALTY EXPERIENCE

- Compliance with Caltrans procedures
- Caltrans electronic lane closure system (LCS)
- Manual of Uniform Traffic Control Devices (MUTCD)
- Materials Testing

SCOPE TECHNICAL AREA EXPERTISE

- Construction Management
- Quality Assurance/Quality Control Inspection
- Planning
- Scheduling
- Structural design

23+

years of experience

3+

years with EXP

PROFESSIONAL REGISTRATIONS

- Professional Engineer (Civil): CA 64709
- CMAA CCM #5798
- OSHA 30

EDUCATION + TRAINING

- MS, Civil Engineering, University of California, Irvine
- BS, Civil Engineering, California Polytechnic State University, San Luis Obispo

AFFILIATIONS + MEMBERSHIPS

- CMAA Southern CA
 - Events & Awards Chair: 2019 - Present
 - CCM Committee Member: 2019
- WTS Orange County
 - Treasurer: 2018 - Present
 - Mentoring Committee: 2016- 2019
- ASCE Orange County
 - Membership Chair: 2000 - 2018

Anh Case has over 23 years of structural and civil engineering experience in the areas of bridges and commercial buildings. Her experience includes construction management, quality assurance/quality control (QA/QC) inspection, planning, scheduling, structural design, quantity takeoffs, cost estimates, plan preparation, and client correspondence. Anh has held positions ranging from Bridge and Construction Inspector to Bridge Engineer, as well as structures representative. She has worked on various transportation projects, including:

- I-405 Improvement Project
- Borden Bridge Road Improvements
- SR 101/Springville Interchange
- SR 178/Fairfax Interchange
- Metro Exposition Transit Corridor
- SR 22 High Occupancy Vehicle (HOV) Widening Design-Build
- Ocean Boulevard/Terminal Island Freeway Interchange

Anh has worked with a variety of agencies, including California Department of Transportation (Caltrans), Orange County Transportation Authority (OCTA), Port of Long Beach (POLB), Metrolink, and various city agencies including Los Angeles, Bakersfield, Camarillo, and San Marcos.

Anh is experienced in the design and construction of simple-span and multi-span bridges, retaining walls, mechanically stabilized earth (MSE) walls, and retrofit of existing bridges in accordance with Caltrans and American Association of State Highway and Transportation Officials (AASHTO) codes. She also has experience with highway construction inspection requiring compliance with Caltrans' procedures, standard plans, and specifications. Anh is very familiar with the Caltrans electronic lane closure system (LCS) and is well versed with the Manual of Uniform Traffic Control Devices (MUTCD), California Supplement. She is also knowledgeable in the required frequencies and procedures of materials testing in accordance with the Caltrans Construction Manual and the California Test Method. Anh understands environmental considerations, including storm water pollution prevention plan compliance.

Software Proficiency: MS Office Suite (Excel, Word, PowerPoint, SharePoint, OneNote), Bluebeam, Adobe Acrobat

Anh Case, PE, CCM – *continued*

Proposed Role: Project Manager | Resident Engineer | Structures Representative

Project Experience

***City of San Marcos, Borden Bridge Road Construction Management, San Marcos, CA**

Anh was the Project Manager, Resident Engineer, and Structures Representative for this project, providing construction management services for the Borden Road Bridge, a gap-closure bridge that spans the San Marcos Creek on Borden Road from Twin Oaks Valley Road to Woodward Street. The project included a two-span pre-cast, pre-stressed T-bulb girder bridge. The bridge was constructed on two types of cast-in-drilled-hole (CIDH) piles and includes two 12-foot traffic lanes in each direction, a five-foot shoulder/bike lane in each direction, a 10-foot wide equestrian/multi-use trail, and a five-foot sidewalk. Roadway improvements included bridge approaches, drainage, landscaping, and irrigation improvements. Responsibilities: Anh reviewed and coordinated comments from all reviewers for working drawings such as falsework plans, column guying plans, pre-stress and pre-cast shop plans, rebar shop plans, and pile drivability study. She ensured that the pile driving activity was monitored continuously and the pile driving records were logged and documented for the CIDH piles. She provided the abutment grades, column grades, camber, soffit grades, and top of deck grades using the bridge deck grades (four-scale). Anh also ensured that embankment surcharge loads were built and monitored per the contract specifications. She was also responsible for addressing requests for information (RFIs) pertaining to the bridge. She led pre-activity meetings to address milestone activities such as pile drilling, falsework erection and removal, superstructure concrete pours, and pre-stressing of the girders. Anh worked closely with the City of San Marcos' material technicians to ensure accurate testing and material acceptance for items used on the project. Other responsibilities included coordinating with utility entities (Vallecitos Water District and San Diego Gas and Electric) through the bridge structure. She provided daily reports, weekly structural newsletters, and weekly statements of working days and photos to ensure accurate documentation of the project. She also provided quantity sheets and reviewed and negotiated pay estimates with the contractor on a monthly basis. She reviewed and negotiated contract change orders (CCOs) and ensured the contractor's compliance with plans, specifications, and other contract documents to produce quality structures while maintaining a safe environment. At the completion of the project, Anh provided closeout items such as certification of materials, disposition of records, and as-built information.

***Orange County Transportation Authority (OCTA), I-405 Improvement Project, Orange County, CA**

Orange County Transportation Authority (OCTA) in cooperation with The California Department of Transportation (Caltrans) is widening the San Diego Freeway (I-405) between State Route 73 (SR-73) and Interstate 605 (I-605). Anh served as the Structures Segment Lead. The project will improve 16 miles of I-405 between the SR-73 freeway in Costa Mesa and I-605 near the L.A. County line. The project includes adding one general purpose lane in each direction between Euclid Street and I-605 and making improvements to freeway entrances, exits and bridges. In addition, the project will add the 405 Express Lanes, incorporating the existing carpool lanes and a new lane in each direction between SR-73 and I-605. The project is within 4 City agencies including Seal Beach, Huntington Beach, Westminster and Costa Mesa and includes improvements to 32 utilities and 22 bridges along the corridor. Improvements to the 22 bridges include widenings, rehabilitation, replacements and stage construction. Anh was the Segment 3 Structures Lead. She was responsible for the oversight of the Caltrans Quality Verification Structures Team in Segment 3. She also manages inspection staff, reviews a variety of submittals such as shoring, falsework, excavation plans, bridge demo plans and concrete mix designs. Anh also review the Quality Management Plans and various construction work plans.

Inland Valley Development Agency (IVDA), Mountain View Avenue Roadway Improvements Project, San Bernardino, CA

The Inland Valley Development Agency (IVDA) is the lead agency for the Mountain View Avenue Roadway Improvements Project. Anh served as the Structures Representative. This \$12.2M project will replace the two-lane bridge at Mission Creek with four lanes, widen Mountain View from the Mission Creek Bridge to the Santa Ana Bridge, provide new curb, gutter and sidewalk to the west side of Mountain View, construct roadway and traffic signals improvements at Victoria and San Bernardino Avenues, realign the access roads to the SCE Mountain View Generating Station, realign and strengthen the walls of Mission Creek and resurface Mountain View Avenue from the Santa Ana River to Mission Creek. The Project will also provide storm drain, water, gas line, and private improvements along Mountain View Avenue. EXP is providing Construction Inspection and Structures Representative Services. Anh is responsible for reviewing plans such as Girder Erection Plan, Contractor Schedule for Structures related activities and structures grades for substructure and super structure elements.

Other Role / Scope of Services Provided include:

- Management and administration of construction contract activities
- Providing cost, schedule, and document control services
- Assist in Changes and Claims resolution
- Providing project progress reports and attend meetings
- Structures Inspection Services
- Bridge deck 4-scales and grades
- Monitoring construction contractor safety compliance and labor code compliance
- Monitoring construction contractor maintenance and record drawing obligations
- Coordination with Redlands Passenger Rail Project

**Work performed at previous firm.*

Ju Kim, PE, QSD

Proposed Role: Senior Inspector



SPECIALTY EXPERIENCE

- Constructability Reviews
- Construction Support
- Diverse Roadway + Structure experience
- Mechanically Stabilized Earth (MSE) Walls

SCOPE TECHNICAL AREA EXPERTISE

- Construction Management
- Project Management
- Construction QA/QC
- Construction Administration

31+

years of experience

2+

years with EXP

PROFESSIONAL REGISTRATIONS

- Professional Engineer (Civil): CA C54269
- Qualified SWPPP Developer (QSD)

EDUCATION + TRAINING

- BS, Civil Engineering, University of California Davis

AFFILIATIONS + MEMBERSHIPS

- CMAA Southern CA
- ACEC

Ju brings 31 years of professional experience in project management and construction management, including 21 years as a California Department of Transportation (Caltrans) senior bridge engineer, and three years as Division Manager of Construction for Orange County Public Works (OCPW) managing county wide construction projects. As a member of the DRB (Disputer Resolution Board) he is well verse in field of Construction Management. His diverse roadway and structure experience includes Caltrans Maintenance Building and Facility, Metro Transit Stations, freeway and roadway construction, various types of bridges: cast-in-place (CIP) box girder bridges, T-girder bridges, pre-cast I girder bridges and steel I-girder bridges, bridge retrofit. Additional experience includes ADA ramps, sidewalk, utilities, various type of structure foundation and retaining walls. Areas of specialty are constructability review, construction support, shop drawing review, submittal review, construction QC/QA, responding to RFI's, SWPPP/BMP, Construction Administration, "as-built" documentation, NOPC and Claim, Contract Change Order. He also has an in-depth knowledge of mechanically stabilized earth (MSE) walls, soil nail walls, tie back walls, stone columns, deep soil mixing. Recently Ju Kim as structure manager on a \$1.2 billion design build contract, managing field and office operation for all structure related items on this contract. Some of his career highlights are the I-105 Century Freeway, reconstruction of the Santa Monica Freeway after the Northridge Earthquake, reconstruction of the Cypress Freeway (San Francisco), I-5 widening (OCTA- Caltrans), San Francisco Bay Bridge Retrofit, Coronado Bridge Retrofit, I-5/805 Interchange, SR-57/60 Interchange, I-5 Gateway (OCTA-Caltrans), 57 Freeway widening (OCTA-Caltrans), San Gabriel Trench Project (SGVCOG) and OCTA 405 Design Build (OCTA). Some of his career highlights include:

- Reconstruction of the Cypress Freeway (San Francisco)
- I-5 widening (OCTA-Caltrans)
- Coronado Bridge Retrofit, I-5/805 Interchange
- SR-57/60 Interchange, I-5 Gateway (OCTA-Caltrans)
- 57 Freeway widening (OCTA-Caltrans)
- San Gabriel Trench Project (SGVCOG)
- OC405 Design Build (OCTA)
- San Francisco Bay Bridge Retrofit
- I-105 Century Freeway, reconstruction of the Santa Monica Freeway after the Northridge Earthquake

Software Proficiency: MS Office (Word, Excel, PowerPoint), and Bluebeam.

Ju Kim, Pe, QSD – *continued*

Proposed Role: Senior Inspector

Project Experience

* Caltrans District 7; I-5 Burbank Blvd OC, Burbank, Los Angeles County, CA

Ju served as the Assistant Structures Representative on this project with two spans: left and right bridge over I-5. Project also included three MSE Walls and Retaining Walls. Responsible for reviewing submittals and field inspection activity. He performed the followings: Surveying, MSE Walls construction, Barrier Slab construction, 24" pipe pile installation operation, PDA monitoring of the piles, generating grades for the followings: footing, abutment wall, retaining wall, falsework, soffit/stem and bridge deck using 4-scale. In addition, he performed the Falsework review, preparing project reports and photographs; preparing quantity calculations and assisting with preparation of the Contractor's progress payment; and assisting with resolving construction issues. Construction Cost: \$20 million. Start/End Dates: October 2020 – April 2021.

* San Gabriel Valley Council of Governments (SGVCOG), Alameda Corridor East Construction Project (ACE), San Gabriel Trench Grade Separation Project, San Gabriel, CA

Ju served as the Lead Bridge Engineer/Structures Representative, supervision of the structure's inspection staff and assigning individual assignment and task. Reviewed and approved contractors work plans and procedures, reviewed and approved shop plans, submittals and RFIs, prepared project reports and maintaining project files, prepared quantity calculations and approved Contractor's monthly progress payment. Negotiated contract change order as and resolved construction claims and project issues.

The project constructs a 1.4-mile-long concrete railroad trench comprised of 4,000 plus CIDH piles and 4,000 plus secant piles, four new grade separations over the lowered UPRR railroad track, and new permanent railroad bridges constructed at the Alhambra and Rubio Washes. The project includes utility relocations to include relocation of fiber optic infrastructure along the UPRR right-of-way, storm drain, sanitary sewer improvement and numerous other utility relocations to include water, gas, electrical and communications, construction of a 2+ mile-long shoofly track, retaining walls, a secant pile wall trench, roadway improvements, traffic signals and street lighting and landscaping. Contract requires challenging coordination with three cities, Caltrans, UPRR, LACDPW, LACSD, US Army Corps of Engineers and numerous utility companies and agencies. Construction cost: \$172 million. Start/End Dates: September 2014 – November 2018.

* Caltrans and OCTA, I-5 Freeway Widening, Buena Park, CA, USA

Ju was the Senior Bridge Engineer/Structure Representative responsible for managing and supervising 10 bridge engineers and reviewing submittals, requests for information (RFIs) and shop drawings. He was also responsible for maintaining accurate project documentation and the project budget.

The project included reconstruction of the I-5 from six to 12 lanes of freeway, including construction of six new bridges, 29 retaining walls and one pump plant. It also replaced four CIP/post-tension box girder bridges, widened one T-girder bridge, built cast-in-steel-shell (CISS) and CIDH piles, waterproofed and jacked the superstructure, retaining walls and architectural features. The project was completed on-time and within budget. Cost: \$206 million. Project Start/End Dates: April 2006 – November 2010.

* Caltrans, SR-57/SR-60 Interchange, Diamond Bar and City of Walnut, CA, USA

Ju served as Senior Bridge Engineer/Structure Representative and was responsible for supervising eight engineers and overseeing the construction and design of lanes and box girder and span bridges. This project constructed 2.4 miles of direct-connect high occupancy vehicle (HOV) Lanes (one lane in both direction) from the SR-57 to the SR-60 using 11-foot diameter, 100-foot-long CIDH (wet hole) piles with the slurry displacement method, two CIP/post-tension box girder bridges and 29 spans bridges. The project also used soil cements, soil nail walls, retaining walls, MSE walls and sound walls and installed wick drains. Cost: \$50 million. Project Start/End Dates: April 2003 – April 2006.

Timothy Kirkley, LSIT

Proposed Role: OE | Utility Coordinator



SPECIALTY EXPERIENCE

- Constructability Reviews
- Construction Support
- Diverse Roadway + Structure experience
- Mechanically Stabilized Earth (MSE) Walls

SCOPE TECHNICAL AREA EXPERTISE

- Construction Project Management
- Traffic Signal Systems
- Land and Construction Surveying
- Inter-government Operations
- Engineering Services

41+

years of experience

2+

years with EXP

PROFESSIONAL REGISTRATIONS

- LSIT - CA 7L003412

EDUCATION + TRAINING

- Various Mathematic and Economic Classes, San Bernardino Valley College, San Bernardino, California
- VAL-107 Risk Management Principles & Practices Certification, Project Management Institute (PMI)

AFFILIATIONS + MEMBERSHIPS

- Construction Management Association of America (CMAA)

Tim has over 41 years of experience in Transportation Engineering. His areas of expertise are within construction project management, traffic signal systems, land and construction surveying, inter-government operations, engineering services and project management. He has performed numerous field surveys to ascertain data for major highway and freeway design and construction projects. He has administered contracts providing photogrammetry materials, including mapping and 3D survey data. Tim also has experience in Construction Project Management, including traffic signal communication systems, freeway landscaping and irrigations projects, sound wall projects, bridge replacement and railroad grade separations.

Software proficiency: MS Office Suite (Word, Excel), Bluebeam Revu.

Project Experience

Inland Valley Development Agency (IVDA), Mountain View Avenue Widening Project Phase II, San Bernardino, CA

Tim served as the Construction Structures Representative with responsibility for construction oversight of the Mission Creek Bridge replacement structure, replacing a two-lane bridge with a four-lane pre-cast girder bridge over Mission Creek, a County Flood Control channel.

Riverside County Transportation Authority (RCTC), SR-60 Truck Lanes Project, Riverside County, CA

Tim served as the Structures Representative, responsible for construction oversight of all structures on the project including retaining walls, arch culverts, box culverts and structure related drainage systems; daily reporting of construction activities; tracking of daily bid item quantities incorporated into the project. The SR-60 Truck Lanes Project will widen a 4.5-mile section of State Route 60 between Gilman Springs Road and 1.4 miles west of Jack Rabbit Trail through unincorporated Riverside County's Badlands between Moreno Valley and Beaumont. The project lies within mountainous terrain with a curving alignment and steep grades.

*San Bernardino County Transportation Authority (SBCTA) Program Management & Other Specialty Services, San Bernardino, CA

As Construction Manager, Tim was responsible for the administration of various types of construction projects including sound wall project, highway landscaping, coordinated traffic signal system, railroad Grade Separations, bridge replacement, interchange improvements and widening projects. Tim directed numerous Construction Management teams, under contract with SBCTA, to oversee the day-to-day activities,

Timothy Kirkley, LSIT – *continued*

Proposed Role: Senior Inspector

acting as the agent for SBCTA, including the following projects:

- *** US 395 Phase 1 Widening; Adelanto, CA:** This project widened 5 miles of US 395 from 2 lanes to 4 with a paved median. This project included simultaneous work activities and coordination with the owner of jet fuel transportation lines within the work area requiring critical scheduling of work by their contractor and SBCTA's contractor while ensuring the project could be completed within budget and on schedule.
- *** I-10 Pepper Avenue Bridge Replacement Project, Colton CA:** This project scope was to replace an existing two-lane bridge with a five-lane bridge with improved shoulders and improvements to freeway ramps and local streets. The project included new traffic signal systems for both the westbound and eastbound ramps and coordination with an adjacent signalized city street. The bridge was constructed in stages allowing full use of the existing bridge during construction. Also included in the construction was a 24-inch water line installed in the bridge structure for the local water district.
- *** I-10 Pepper Avenue Bridge Replacement Project, Colton CA:** This project scope was to replace an existing two-lane bridge with a five-lane bridge with improved shoulders and improvements to freeway ramps and local streets and was constructed in stages allowing full use of the existing bridge during construction. The project included new traffic signal systems for both the westbound and eastbound ramps and coordination with an adjacent signalized city street. Also included in the construction was a 24-inch water line installed in the bridge structure for the local water district.
- *** Palm Avenue Railroad Grade Separation Project, San Bernardino, CA:** This project was to replace an at-grade crossing of the BNSF rail lines by constructing a bridge structure providing a grade separation from vehicular traffic from train traffic. This project required coordination with BNSF for work required over their rail lines to eliminate delays to the project.
- *** San Bernardino Valley Coordinated Traffic Signal System (SBVCTSS), San Bernardino Valley Local Jurisdictions, CA:** The SBVCTSS, "project" involved the coordination of over 1,250 traffic signals in 15 local jurisdictions on major arterial corridors throughout the San Bernardino Valley. The project was constructed using a 4-tier approach over a 5-year period at a cost of over \$16 million. The project yielded significant reduction in travel times, decrease in fuel costs while reducing emissions.
- *** Various Landscaping Project; San Bernardino Valley, CA:** SBCTA, through agreements with the California State Department of Transportation (Caltrans), constructed eleven landscaping projects on I-10, I-215 and SR-210. These projects included installation of irrigation systems, trees, scrubs and ground cover plant material, wood mulch and boulder and gravel mulch.

***Expo Light Rail Blue Line Project – Design/Build; Los Angeles, CA**

This design-build project was to construct 8.6 miles of light rail transit line with 12 stations between downtown Los Angeles and Culver City. As the Configuration Manager, Tim was responsible for tracking all changes to the design and the proper administration of current project plans for availability to the contractor.

***SR-22 Garden Grove Freeway Improvement Project – Design/Build; Orange County, CA**

This \$550 million design-build project was to improve 12-miles of the SR-22 in the cities of Westminster, Garden Grove, Santa Ana and Orange. The project included 2 railroad grade separations, one freeway-to-freeway interchange, 35 over-and under-cross bridges, one HOV carpool lane in each direction, auxiliary lanes, an elevated connector, collector-distributor roads, over 70 retaining walls, sound walls and landscaping improvements. Acting as Assistant Structures Representative, Tim was responsible for oversight of construction of various bridges, sound walls, retaining walls and drainage structures.

Arvin K. Chaudhary, PE, PLS

Proposed Role: Surveyor



SPECIALTY EXPERIENCE

- Private & Public Works Projects
- Highways/Freeways/Bridges/Railroads

SCOPE TECHNICAL AREA EXPERTISE

- Experienced in all facets of project delivery
- 18 years' experience as a consultant contract manager

33+

years of
experience

30+

years with SCA

PROFESSIONAL REGISTRATIONS

- Professional Engineer (Civil): CA 5400
- Land Surveyor #7857

EDUCATION + TRAINING

- BS, Aerospace Engineering, 1987, Boston University

AFFILIATIONS + MEMBERSHIPS

- Caltrans Small Business Council
- Council of Professional Surveyors

Arvin has over 32 years of experience including 18 years as a consultant contract manager on Caltrans contracts. His responsibilities have included all facets of project delivery from initial funding studies to post-construction right of way closeout, project management, permitting coordination, surveying, site design, utility relocation preparation of drawings and specifications, construction management, construction inspection, and expert witness testimony. Projects have included residential developments, highways, freeways, bridges, commercial centers, treatment plants, utility systems, hospitals, schools, airports, and railroads. He currently sits on the Caltrans Small Business Council, Council of Professional Surveyors, and has previously served on committees and task forces regarding Prevailing Wage laws, Federal Acquisition Regulation A&E Audits and Audit Compliance, ARRA-funded project tracking, Local Agency Public Works fee structuring, and the Workforce Investment Board.

Project Experience

Caltrans Contract No. 07A4445 On-Call Land Surveying Services District 7, Southern Los Angeles County, CA

Contract Manager for on-call surveying services to Caltrans District 7 in the Southern part of the County of Los Angeles. Task Orders to date have included: Photogrammetry control and land net surveys for a concrete barrier replacement along the LA-010 corridor near Mid-City; Horizontal and vertical control for 33 miles along the LA-001 corridor between Long Beach and Redondo Beach; Conventional topographic surveys of intersections for 85 ADA ramp improvement projects; and Lidar scanning and complete Civil 3D DTM deliverables for 12 intersections for ADA ramp and intersection improvements.

Los Angeles County On-Call Surveying Services, Fire Camp 8, Los Angeles County, California

Project Manager for design and construction surveys on an as-needed basis. Services will include aerial photogrammetry, topography, hydrography, dam monitoring, and project area LiDAR scanning. Construction surveys will include road construction, upgrading of existing drainage facilities, establishing new drainage facilities, staking sewer and water lines, and monument preservation. Deliverables will be made in MicroStation/Inroads. Current task order is right of way and design surveys for Los Angeles County Fire Camp 8 to install 1500 feet of water pipeline for re-filling of water dropping helicopters. Survey was done by both aerial and conventional survey methods and included full Digital Terrain Model.

Arvin K. Chaudhary, PE, PLS – *continued*

Proposed Role: Surveyor

Caltrans Contract 07A4045, Los Angeles, CA

Contract and Task Order Manager for on-call survey services. To date task orders have included design surveys for Caltrans on Route 101 between 4th Street and Los Angeles Street in Downtown Los Angeles as well as design surveys for Caltrans on Interstate 110 for 24 miles in south Los Angeles County. Conventional and GNSS integrated survey to Caltrans standards for proposed ADA ramp improvements.

Anaheim Canyon Station Improvement Project, Orange County, Anaheim, CA

Project Manager for construction of a multi-modal transit center that will accommodate expanding Metrolink commuter rail service, OCTA, local and community bus services, Stationlink rail feeder service and Anaheim Resort Transit. Chaudhary & Associates provided GPS control survey, and topographic services.

55TH Way Landfill, Los Angeles County, Long Beach, CA

Project Manager for construction layout and survey of the 55th Way Landfill site in Long Beach for conversion to a public park. Quantity and quality control surveys at various stages of construction per City of Long Beach and Los Angeles County standards.

Caltrans Contract 06A2376 On-Call Surveying, Fresno, Tulare, Madera, Kings, and Kern Counties, CA

Contract Manager for \$3.7M On-Call Land Surveying Contract. Task Orders have included staff augmentation, LiDAR surveys, and conventional surveys. Specific tasks have included setting control and preparing control reports, static and RTK GPS, digital leveling, conventional total station surveys using Caltrans TSS codes, mobile and stationary LiDAR scanning, point cloud processing, civil 3-D DTM's, Microstation CADD files, and preparing survey binders.

Contract No. 06A2332, On-Call Surveying District 10, Mariposa, Merced, San Joaquin, Stanislaus, Tuolumne, Alpine, Amador, and Calaveras Counties, CA

Contract and Task Order Manager for on-call contract covering nine counties and five survey office locations. Task Orders involved up to 16 staff in the field and office providing augmentation for design and construction surveying, monument recovery, office CADD support using Microstation and Civil 3D, Trimble Office, Lidar data sets, Cyclone, and rendering and animation visualizations.

David Hamilton, PE, GE

Proposed Role: Geotechnical Engineer



SPECIALTY EXPERIENCE

- Shallow and deep foundation design
- Geologic hazard evaluations
- Evaluation of site geologic and groundwater conditions

SCOPE TECHNICAL AREA EXPERTISE

- Geotechnical Engineering
- Construction Observation and Testing

22+

years of
experience

17

years with H+A

PROFESSIONAL REGISTRATIONS

- Professional Engineer (Geotechnical): CA 2721
- Professional Engineer (Civil): CA 56590

EDUCATION + TRAINING

- BS, Civil Engineering, Loyola Marymount University
- MS, Geotechnical Engineering, University of California at Berkeley

AFFILIATIONS + MEMBERSHIPS

- Executive Board of Directors, California Geotechnical Engineers Association (CalGeo)
- American Society of Civil Engineers (ASCE)

David has more than 22 years of broad engineering experience involving geotechnical and geological investigations for heavy civil construction, foundations, complex industrial, infrastructure and public works projects. David is on the Executive Board of Directors for the California Geotechnical Engineers Association (CalGeo) and has served as an expert to the State of California Board of Professional Engineers, including Geotechnical Engineer licensure and standard of practice. David also has previous experience as a Professor of Soil Mechanics and Foundations as well as Solid Waste Engineering in both the undergraduate and graduate civil engineering programs at Loyola Marymount University. He has extensive experience in shallow and deep foundation design, geologic hazard evaluations, landfills, landslide and slope stability evaluations, evaluation of site geologic and groundwater conditions and construction testing and inspection.

David has served as the Project Manager and/or Principal Geotechnical Engineer for over twenty (20) projects for Los Angeles County Sanitation Districts (LACSD), including sewer trench, jacking, tunneling, pump station and treatment plant infrastructure. Furthermore, as an employee of LACSD, David was the site geotechnical engineer for the 1995-2000 development of the LACSD Puente Hills MSW Landfill 200-Acre Expansion Project.

Project Experience

Los Angeles County Sand Districts, 1.5-mile, 10-ft diameter Tunnel, Carson and Wilmington Districts, City of Los Angeles, CA

Engineering support for construction of a 10-foot-diameter 1.5-mile long tunnel for 8-foot-diameter RCP sewer. Tunnel crossing within the Alameda Transportation Corridor, Port of Los Angeles railroads, Tesoro Oil Refinery, numerous oil pipelines, and landfills. Provided engineering support during tunnel construction as part of CM team. Tasks included: a) collecting soil/rock samples from tunnel face; b) detailed logging of tunnel rib and lagging systems; c) tunnel ground settlement measurements; d) performance evaluations of existing nearby refinery infrastructure foundations and pipelines, relative to tunneling operations. Assisted in negotiations with Tunneling Contractor. Initial scope included drilling, soil sampling and logging of 50 exploratory borings and advancing Cone Penetration Test (CPT) soundings, assessment of pipeline alignment stratigraphy, material property and characterization.

David Hamilton, PE, GE – *continued*

Proposed Role: Geotechnical Engineer

Los Angeles County Sanitation Districts, Sewer Rehabilitation, Long Beach, CA

Project included geotechnical investigation for rehabilitation of approximately 10,400 feet of 63-inch diameter, semi-elliptical sewer by slip-lining with 57-inch, non-circular, fiberglass reinforced plastic liner pipe, as well as repair of four manholes. This project restored the structural integrity of the trunk sewer, which had moderate to severe structural damage resulting from hydrogen sulfide corrosion. This pipeline is near-parallel to east and south of Freeways 710 and 91, respectively. Geotechnical field exploration and testing included drilling 7 soil borings hollow-stem-auger borings, recording field SPT blow counts, performing geotechnical laboratory testing, and preparing a Geotechnical Data Report (GDR).

Los Angeles County Sanitation Districts, Tunnel in Granite under I-210 Freeway, La Canada Flintridge, CA

Geotechnical investigation and engineering support for construction of a 6-foot-diameter 10,000-foot-long sewer tunnel beneath Freeway 210, in granite bedrock, including a 72-inch-diameter 0.75-inch-thick steel casing as the initial support of the proposed tunnel. Geotechnical field exploration for tunnel, including drilling three continuous rock coring holes in granite bedrock and two soil borings; geotechnical laboratory testing, and preparation of Geotechnical Feasibility Report (GFR). Evaluated ground conditions for a 40 x 20 feet insertion shaft, a 35 x 15 extraction shaft, backfilling of shaft over-excavations to the desired grade, tunnel excavation, methods for Tunnel Boring Machine (TBM) and drill-and-blast, construction risk considerations, and recommendations for monitoring during construction.

Los Angeles County Sanitation Districts, Joint Outfall “A” Unit 6 Trunk Sewer Rehabilitation, Long Beach, CA

The Project included the rehabilitation of approximately 10,400 feet of 63-inch diameter, semi-elliptical sewer by slip-lining with 57-inch, non-circular, fiberglass reinforced plastic liner pipe. The project also includes repair of four manholes. The project would restore the structural integrity of the trunk sewer, which has moderate to severe structural damage resulting from hydrogen sulfide corrosion. This pipeline is near-parallel to Freeways 710 and 91 to the east and south of them, respectively. Geotechnical field exploration and testing for a 5-foot-diameter sewer pipeline.

Los Angeles County Sanitation Districts, Joint Outfall “C” Unit 3G Force Main No. 2, Signal Hill, CA

The Project consisted of the construction of approximately 6,100 feet of 12-inch diameter high-density-poly-ethylene (HDPE) force main and vitrified clay pipe (VCP) sewer and appurtenant structures. Geotechnical field exploration and testing for a sewer pipeline, including drilling eleven (11) hollow-stem-auger borings and three (3) bucket-auger, respectively 6-inch and 24-inch in diameter. Testing included recording SPT-N values in the field and performing laboratory geotechnical testing. A geotechnical data report (GDR) was prepared.

LACSD Union Street Trunk Sewer, Section 1, Inglewood, CA

The project consisted of replacing the existing Slauson Avenue Pumping Plant and Force Main, which were constructed in 1964. The project consisted of the construction of approximately 1,100 feet of 18-inch-diameter sewer, open-trench, micro tunneling vitrified clay (VC) or fiber-reinforced pipe (FRP), and jacking/tunneling approximately 320 feet long, and 5 feet under the base of the existing Rio Hondo flood control channel (42-inch-diameter steel casing), appurtenant structures and jacking/extraction pits, for a maximum soil cover of 28 feet.

Kim Morales

Proposed Role: Senior Labor Compliance Analyst



SPECIALTY EXPERIENCE

- LC monitoring
- LC audits
- Handling LC violations
- Monitoring apprentice ratios
- Fringe benefit reviews

SCOPE TECHNICAL AREA EXPERTISE

- Labor compliance/ prevailing wage monitoring

8+

years of
experience

4+

years with
GCAP

Education + Training

- OCEMS EMT Accreditation, EMT Certificate, Santiago Canyon College Classes

Kim is a highly organized and detail-oriented analyst with a broad range of construction industry experience. She has a thorough understanding of labor compliance related rules and regulations. Kim has experience monitoring and ensuring proper wages are paid and apprentice ratios are accurate. Kim has worked with subcontractors and client auditors to resolve labor compliance related issues. Kim's responsibilities at GCAP include certified payroll review for major public works transportation projects, field interviews, and compliance reporting.

Project Experience

Orange County Transportation Authority, I-405 Improvement Project, Costa Mesa, CA

Kim serves as the Senior Labor Compliance Analyst for this project. She conducts jobsite labor compliance and commercially useful function interviews. She is responsible for the review, analysis, and organization of labor compliance related documents such as jobsite interview reconciliation and certified payroll document management. She generates reports and provides status updates.

Riverside County Transportation Commission, I-15 Express Lanes Project, Riverside, CA

Kim serves as the Senior Labor Compliance Analyst for this project. Her duties include performing weekly review of certified payroll reports submitted by contractor and subcontractors as well as performing monthly reviews of Training contributions and Fringe Benefits. Kim conducts Apprentice ratio monitoring and confirms Apprentice Certifications (Federal and State). She is responsible for the review, analysis, and organization of labor compliance related documents such as jobsite interview reconciliation and certified payroll document management. She generates reports and provides status updates. She is responsible for verifying proper reporting by Owner-Operators and conducting jobsite interviews on all project locations.

San Bernardino County Transportation Authority, I-10 Corridor Project, San Bernardino, CA

Kim serves as the Labor Compliance Analyst for this project. She is responsible for conducting audits of payroll records and other related documents for inconsistencies, discrepancies or other issues to ensure compliance with the payment of prevailing wage rates. She monitors workforce utilization reports to verify trade and apprenticeship participation. Kim prepares the required reports and interfaces with contractors to resolve discrepancies.

Kim Morales – *continued*

Proposed Role: Senior Labor Compliance Analyst

Los Angeles County Metropolitan Transportation Authority, Westside Subway Extension (aka “Purple Line Extension”), Los Angeles, CA

Kim serves as the Labor Compliance Analyst for this project. She uses Metro's on-line “Prevailing Wage Reporting System” (LCP tracker) to oversee timely submittal of certified payroll reports and other related documents to ensure prime contractor and subcontractor(s) compliance. Kim conducts on-site interviews to determine wages paid to workers. She monitors project apprentice ratios and ensures correct fringe benefits are paid. Kim resolves all labor compliance related issues with subcontractors and client auditor. She monitors and ensures project workers are receiving the correct prevailing wage amount as determined by the DIR, Davis Bacon Act, and Labor Unions. She identifies wage underpayments through audits of payroll documents, calculate and recommend the amount of wages, training funds and/or penalties that Metro will withhold from the contractor's payments/retention.

San Bernardino County Transportation Authority, On-Call Labor Compliance Services, San Bernardino, CA

Kim serves as the Senior Labor Compliance Analyst for this on-call contract which covers a variety of projects including light rail, grade separation, and freeway expansion work. She responds to technical questions regarding prevailing wage requirements for SBCTA projects and monitors to ensure that certified payroll records are submitted by CM firms. She is responsible for conducting audits of payroll records and other related documents for inconsistencies, discrepancies or other issues to ensure compliance with the payment of prevailing wage rates. She monitors workforce utilization reports to verify trade and apprenticeship participation, she prepares the required reports and performs quarterly labor compliance audits.

Eric Turner

Proposed Role: Biologist



SPECIALTY EXPERIENCE

- Regulatory Permitting 401,404,408,1602
- CEQA and NEPA documents

SCOPE TECHNICAL AREA EXPERTISE

- Environmental Planner
- Construction Management
- Regulatory Permits

11+

years of
experience

6+

years with VCS

Education + Training

- MS, Urban & Regional Planning, emphasis in Environmental Planning & Policy, Cal Poly Pomona
- BA, History, University of California, San Diego

Affiliations + Memberships

- Member, Association of Environmental Professionals

Eric has been working with interdisciplinary teams in support of public and private commercial, residential, transportation and infrastructure projects in the Southern California area since 2010. Duties include preparing and processing environmental documents in compliance with the California Environmental Quality Act (CEQA) and the National Environmental Policy Act (NEPA); and preparing and processing regulatory permits with the USACE, RWQCB and CDFW. Eric also conducts research on local, state, and federal agency policies and protocols; prepares environmental analyses; oversees technical consultants and document production; translates technical documents for public and decision-maker understanding; and prepares scopes of work, schedules, and budgets. Prior to joining VCS, Eric worked for the City of San Diego's Environmental Services Department reviewing CEQA documents and preparing waste management plans. Eric's main strength is his ability to foresee potential obstacles early in the project scoping phase and identify appropriate actions to take in advance to maintain project schedules and budgets.

Project Experience

SBCTA, I-10 Cherry and I-10 Citrus Avenue Interchanges, San Bernardino, CA

Eric reviewed the Environmental Commitments Records (ECRs) for both the I-10 Cherry Avenue Interchange and I-10 Citrus Avenue Interchange roadway improvement projects for SBCTA. Eric was responsible for obtaining all documentation to demonstrate and confirm project compliance. Eric coordinated with SBCTA staff, project engineers, contractors, consultants, and reviewed technical documents, plans, and specifications to acquire the needed documentation for ECR compliance on both projects.

SBCTA, SR-210 Pepper Bridge Replacement Project, Rialto, CA

This project obtained CEQA and NEPA clearance through an Initial Study / Environmental Assessment approved by Caltrans in 2014. Since 2014, the project underwent a design change to relocate a proposed detention basin. The basin relocation triggered Caltrans' environmental document revalidation process. Eric worked with VCS staff, subconsultants and Caltrans staff to revalidate the project's environmental documents including the Initial Site Assessment (ISA) and Scoping Questionnaire for Water Quality Issues (SQWQI). Eric also processed applications with regulatory agency staff to verify use of a non-notifying Nationwide Permit 14, procure the 401 Certification, and obtain an OpLaw 1602 Agreement from CDFW.

Eric Turner – *continued* Proposed Role: Biologist

SBCTA, Laurel Street Grade Crossing, Colton, CA

The Laurel Street Grade Crossing project in the City of Colton creates a potential sump condition during storm events that requires an emergency generator to operate pumps to prevent flooding. The project site is located in an area with businesses and single-family residences. Eric represented SBCTA and the City of Colton and obtained the ICE permit from SCAQMD to allow construction and operation of the generator.

Murrieta Valley Unified School District, CNG Bus Conversion IS/MND, Murrieta, CA

The Murrieta Valley Unified School District (MVUSD) compressed natural gas (CNG) Bus Conversion Project entailed conversion of an existing diesel fleet to run on CNG. Eric conducted preliminary research and coordinated with consultants on Noise, Air-Quality, and Biological Resources for the creation of the technical studies in support of the Initial Study. He also co-wrote the Initial Study/Mitigated Negative Declaration (IS/MND) and prepared the notifications and filings in accordance with CEQA guidelines.

City of Lake Elsinore, East Lake Specific Plan, Lake Elsinore, CA

VCS prepared the Environmental Impact Report (EIR) and supported the City of Lake Elsinore with development of the East Lake Specific Plan. The 2,950-acre project area, located in the Lake Elsinore Back Basin, included multiple planning areas to accommodate mixed-uses that would allow for industrial logistic warehousing, commercial and residential uses. This area is particularly challenging due to the local hydrology, presence of environmental resources, complex regulatory requirements and multiple landowners within the planning area. Because of the unique nature of this project area, VCS has worked closely with stakeholders throughout the project. Primary environmental issues associated with the project included traffic impacts on local and regional roadways, land uses, hydrology/water quality, air quality, noise, biology and regulatory permitting.

SBCTA, I-215 University Interchange, San Bernardino, CA

Environmental oversight for SBCTA. This project will reconstruct the I-215 and University Avenue Interchange to create the first Diverging Diamond Interchange (DDI) design implemented in California. As part of the Project Delivery Team (PDT), Mr. Turner provided oversight and peer review over the scoping and production of technical studies including a Natural Environmental Study/Minimal impacts (NESMI), Phase I Initial Site Assessment (ISA), Historical Resources Evaluation Report (HRER), Archaeological Survey Report (ASR), Historic Property Survey Report (HPSR) and Visual Impact Assessment (VIA) technical memo during the project's PA&ED phase.

Rialto Unified School District, CNG Fueling Station & Transportation Yard, Rialto, CA

This project entailed development of an approximate 6.5 acre site to be used as a CNG commercial and public fueling station and transportation yard for maintenance and storage of the District's existing and build-out bus fleet. Eric wrote and processed the Initial Study / Mitigated Negative Declaration and managed and reviewed technical studies for biology, cultural, air quality/GHGs, hazards, traffic, water quality and noise. Unique challenges faced on this project included capturing representative traffic conditions, site preparation for removal of existing construction debris stockpiles and attenuation of noise impacts on nearby residential housing. The City of Rialto approved the project and certified the environmental document in February 2015. After project approval, Eric prepared and submitted a grant application to the California Energy Commission, resulting in \$500,000 of reward money for phase I of project construction.

PROJECT EXPERIENCE



INLAND VALLEY DEVELOPMENT AGENCY (IVDA), MOUNTAIN VIEW AVENUE ROADWAY IMPROVEMENTS PROJECT SAN BERNARDINO, CA

Inland Valley Development Agency (IVDA) is the lead agency for the Mountain View Avenue Roadway Improvements Project. This project will replace the two-lane bridge at Mission Creek with four lanes, widen Mountain View from the Mission Creek Bridge to the Santa Ana Bridge, provide new curb, gutter and sidewalk to the west side of Mountain View, construct roadway and traffic signals improvements at Victoria and San Bernardino Avenues, realign the access roads to the SCE Mountain View Generating Station, realign and strengthen the walls of Mission Creek and resurface Mountain View Avenue from the Santa Ana River to Mission Creek. The Project will also provide storm drain, water, gas line, and private improvements along Mountain View Avenue. EXP provided Construction Inspection, Structures Representative Services, Schedule Support, and Claims Support for this project.

Timeline: Completed 2020

KEY PERSONNEL

- Ju Kim, PE (EXP)
- Anh Case, PE, CCM (EXP)
- Timothy Kirkley, LSIT (EXP)
- Eric Turner (VCS)

RELEVANT SERVICES PROVIDED

- Construction Inspection
- Structures Representative
- Schedule Support
- Claims Support



SAN GABRIEL VALLEY COUNCIL OF GOVERNMENTS (SGVCG), TURNBULL CANYON ROAD GRADE SEPARATION

San Gabriel Valley Council of Governments (SGVCOG) is the lead agency for this Project. The project will separate the roadway from the Union Pacific Railroad tracks on Turnbull Canyon Road in the City of Industry/County of Los Angeles with the construction of a two-lane roadway overpass at Turnbull Canyon Road between Salt Lake Avenue on the north and Clark Avenue on the south.

The project will involve construction of a Pedestrian over head Bridge providing pedestrian access for Turnbull Canyon Road over the UPRR Tracks. The project also includes the replacement of the San Jose Creek Bridge which will construct a widened bridge with a longer span to provide a bike lane beneath the bridge. Timeline: 2020-2021.

KEY PERSONNEL

- Anh Case, PE, CCM (EXP)
- Timothy Kirkley, LSIT (EXP)

RELEVANT SERVICES PROVIDED

- Constructability Review
- Cost Estimation
- Schedule Support



RIVERSIDE COUNTY TRANSPORTATION COMMISSION (RCTC), SR 60 TRUCK CLIMBING LANE PROJECT, RIVERSIDE, CA

The SR-60 Truck Lanes Project will widen a 4.5-mile section of State Route 60 between Gilman Springs Road and 1.4 miles west of Jack Rabbit Trail through unincorporated Riverside County's Badlands between Moreno Valley and Beaumont. The project lies within mountainous terrain with a curving alignment and steep grades. The project is designed to improve safety and efficiency and reduce traffic congestion.

Project features include: constructing an eastbound truck climbing lane; constructing a westbound truck descending lane; widening shoulders to standard widths 12-foot exterior shoulders and 11-foot interior shoulders; establishing 2 wildlife crossings and placing wildlife fencing; improving motorist sight distance by flattening roadway curves.

Timeline: 2018-2020.

KEY PERSONNEL

- Ju Kim, PE (EXP)
- Timothy Kirkley, LSIT (EXP)

RELEVANT SERVICES PROVIDED

- Structure Inspection
- Roadway Inspection

Staffing Plan

		Jan 2022	Feb 2022	Mar 2022	Apr 2022	May 2022	June 2022	July 2022	Aug 2022	Sept 2022	Oct 2022	Nov 2022	Dec 2022	Jan 2023	Feb 2023	Mar 2023	Apr 2023	May 2023	June 2023	July 2023	Aug 2023	Sept 2023	Oct 2023	Nov 2023	Dec 2023	Jan 2024	Feb 2024	Mar 2024	Apr 2024	May 2024	June 2024	July 2024	Aug 2024	Sept 2024	Total Hours				
Project Phase		Constructability Review			Advertise Bid		Bid Opening					Contractor NTP + Start of Construction																				Project Completion					Close out +Project Handover		
Total Hours (By Project Phase)		120			0		228					8,142																				240							
Name	Role																																						
Anh Case, PE, CCM	PM / RE / SR	20	20	5			40				40	80	80	80	80	160	160	160	160	120	120	100	100	100	100	80	80	60	60	40	40	40	50	50	2,225				
Ju Kim, PE	Sr. Inspector	15	15	5			20				20	80	120	160	160	160	160	160	160	160	160	160	120	120	120	120	100	100	80	80	70	40	20	2,845					
Tim Kirkley	OE/Utilities Coordinator	20	15	5			40				40	50	80	80	80	80	60	60	60	60	60	60	60	60	60	60	50	40	40	40	40	40	40	40	1,440				
Arvin Chaudhary, PE, PLS	Surveyor										4			60	60	60	60	50	40	40	40	40	40	40	40	40	40	40	40	20					754				
David Hamilton, PE, GE	Materials Tester										4				70	70	70	60	30	20	20	60	60	60	60	60	60	20	60					844					
Kim Morales	Labor Compliance										4		25	25	25	25	25	25	25	25	25	25	25	25	25	25	25	25	25	25	25	25	25			504			
Eric Turner	Environmental										16			8	8	8	8	8	8	8	8	8	6	6	6	6	6								118				

Legend

Full Time Hours

Part Time Hours

Total Hours: 8,730



COST PROPOSAL

NOTE: THIS HAS BEEN SUBMITTED UNDER A SEPARATE COVER AS PER THE RFP REQUIREMENTS.

6

MBE/WBE/DBE UTILIZATION

6 MBE/WBE/DBE UTILIZATION

EXP UNDERSTANDS THAT MBE/WBE/DBE PARTICIPATION FOR THIS PROFESSIONAL SERVICES CONTRACT IS REQUIRED. AS PART OF OUR DIVERSITY, EQUITY + INCLUSION GOALS, WE ARE COMMITTED TO MEETING AND/OR EXCEEDING THESE REQUIREMENTS.

12%

CITY OF CARSON'S
DBE GOAL

12.4%

EXP'S UTILIZATION OF
DBE SUBCONTRACTORS

The EXP team will comply with the City of Carson MBE/WBE/DBE Utilization requirements in the following percentages:

SUBCONTRACTOR	QUALIFICATION	UTILIZATION
Chaudhary + Associates	DBE	7.5%
GCAP	DBE	3.3%
VCS Environmental	DBE	1.6%
TOTAL		12.4 %

Note: EXP's final subcontractor, Hamilton and Associates, does not qualify under MBE/WBE/DBE, however they qualify as a SBE.

AT EXP, WE VALUE DIVERSITY, EQUITY AND BELONGING. WITH 3,500+ PROFESSIONALS WORLDWIDE, WE ARE COMMITTED TO ENSURING OUR FIRM BETTER REFLECTS THE COMMUNITIES WE SERVE THROUGH DIVERSE, EQUITABLE AND INCLUSIVE INITIATIVES.



CLIENT REFERENCE LIST

CLIENT REFERENCE LIST

In order to more fully evaluate your background and experience for the project herein proposed, it is requested that you submit a list of references and/or similar projects completed or in progress within the last 24 months or as noted in the requirements of the RFP or IFB. Your cooperation in this matter is greatly appreciated.

Company Name: EXP U.S. Services Inc.

Number of years as a contractor in the work of this type: 3 years in California

Three projects/clients references for this type of work:

Client Name Advanced Civil Technologies (ACT / Inland Valley Development Agency (IVDA)

Address 6 Hutton Centre Drive, Suite 450, Santa Ana CA, 92707

Contact Name Karen Cohoe, PE, PMP

Contact Email kcohoe@advancedciviltech.com

Contact Phone Number 1.714.253.3645 | m: 1.949.294.4402

Project Description Mountain View Street Improvements

Widen Mountain View Avenue from Mission Creek to the Santa Ana River including replacing the existing bridge crossing Mission Creek Channel.

Project Start Date July, 2020

Project End Date September, 2021

Project Amount: \$12.2M

Client Name San Gabriel Valley Council of Governments

Address 4900 Rivergrade Road, Suite A120, Irwindale CA, 91706

Contact Name Rene Coronel, PE

Contact Email rcoronel@sgvcog.org

Contact Phone Number 1.626.373.2691 | m: 1.626.543.4914

Project Description Turnbull Grade Separation - Construct three bridges, Turnbull Canyon Overpass and Pedestrian Overhead Bridge over the UPRR track and replacement of the San Jose Creek Bridge

Project Start Date September, 2020

Project End Date In Progress

Project Amount: \$45M

Client Name Riverside County Transportation Commission

Address 4080 Lemon Street, 3rd Floor, Riverside, CA 92501

Contact Name Bryce Johnston

Contact Email bjohnston@rctc.org

Contact Phone Number 1.951.787.7141

Project Description Widen 4.5 miles of State Route 60 in Riverside County to provide for truck lanes in each direction and widen shoulders to improve safety and efficiency by reducing traffic congestion.

Project Start Date September, 2019

Project End Date In Progress

Project Amount: \$95.9M

Bidder's Signature 

NOTE: If requested by the City, the bidder shall furnish a certified financial statement, references, and other information sufficiently comprehensive to permit an appraisal of his current financial condition.

CLIENT REFERENCE LIST

In order to more fully evaluate your background and experience for the project herein proposed, it is requested that you submit a list of references and/or similar projects completed or in progress within the last 24 months or as noted in the requirements of the RFP or IFB. Your cooperation in this matter is greatly appreciated.

Company Name: Chaudhary & Associates, Inc.

Number of years as a contractor in the work of this type: 45

Three projects/clients references for this type of work:

Client Name County of Los Angeles

Address 900 S. Fremont Avenue, 5th Floor, Alhambra, CA 91803

Contact Name Ana Lopez

Contact Email anlopez@dpw.lacounty.gov

Contact Phone Number (626) 458-2533

Project Description On-call mapping and land surveying contract which covers all of Los Angeles County. Services included setting control, static and RTK GPS, digital leveling, conventional total station surveys, stationary LiDAR scanning, point cloud processing, Autocad Civil 3-D & Microstation DTM's, CADD files, and utility locating services.

Project Start Date 2015

Project End Date 2021

Project Amount: \$500,000

Client Name Caltrans District 10 - Contract 06A2332

Address 2015 E. Shields Avenue, Suite 100, Fresno, CA 93726

Contact Name Anita Montano

Contact Email anita.montano@dot.ca.gov

Contact Phone Number (559) 908-3169

Project Description On-Call contract. Tasks included setting control, preparing control reports, static and RTK GPS, digital leveling, conventional total station surveys using Caltrans TSS codes, mobile and stationary LIDAR scanning, point cloud processing, civil 3-D DTM's

Project Start Date 2017

Project End Date 2020

Project Amount: \$3.9M

Client Name Caltrans District 7 - Contract 07A4445

Address 100 S. Main Street, Los Angeles, CA 90012

Contact Name Rita Branch

Contact Email rita.branch@dot.ca.gov

Contact Phone Number (213) 897-8900

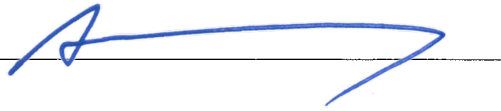
Project Description On-call surveying services in the Southern part of the County of Los Angeles. Task Orders included land net surveys, horizontal and vertical control, conventional topographic surveys, and LiDAR scanning.

Project Start Date 2018

Project End Date 2020

Project Amount: 2.6M

Bidder's Signature



NOTE: If requested by the City, the bidder shall furnish a certified financial statement, references, and other information sufficiently comprehensive to permit an appraisal of his current financial condition.

CLIENT REFERENCE LIST

In order to more fully evaluate your background and experience for the project herein proposed, it is requested that you submit a list of references and/or similar projects completed or in progress within the last 24 months or as noted in the requirements of the RFP or IFB. Your cooperation in this matter is greatly appreciated.

Company Name: GCAP Services, Inc.

Number of years as a contractor in the work of this type: 23 years

Three projects/clients references for this type of work:

Client Name Los Angeles County Metropolitan Transportation Authority

Address One Gateway Plaza, Los Angeles, CA 90012

Contact Name Paula Jurado

Contact Email juradop@metro.net

Contact Phone Number (213) 922-7669

Project Description On-Call Labor Compliance Monitoring Bench, TO 71 Sound Wall #11 Construction

Project Start Date December 2018

Project End Date February 2022

Project Amount: \$108,173.89

Client Name Los Angeles Department of Public Works

Address 900 South Fremont Avenue, Alhambra, CA 91803

Contact Name Robert Eisenhamer

Contact Email Reisenha@dpw.lacounty.gov

Contact Phone Number (626) 458-3155

Project Description Labor Compliance Services, Del Mar Bridge Replacement

Project Start Date September 2015

Project End Date October 2020

Project Amount: \$2,000,000

Client Name San Bernardino County Transportation Authority

Address 1170 W. 3rd Street, 2nd Floor, San Bernardino, CA 92410

Contact Name Jeffrey Hill

Contact Email jhill@gosbcta.com

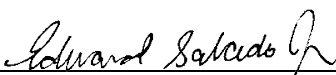
Contact Phone Number (909) 884-8276

Project Description Labor Compliance and Audit Procedures

Project Start Date June 2013

Project End Date July 2019

Project Amount: \$650,000

Bidder's Signature 

NOTE: If requested by the City, the bidder shall furnish a certified financial statement, references, and other information sufficiently comprehensive to permit an appraisal of his current financial condition.

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CLIENT REFERENCE LIST

In order to more fully evaluate your background and experience for the project herein proposed, it is requested that you submit a list of references and/or similar projects completed or in progress within the last 24 months or as noted in the requirements of the RFP or IFB. Your cooperation in this matter is greatly appreciated.

Company Name: Hamilton & Associates

Number of years as a contractor in the work of this type: 17 years

Three projects/clients references for this type of work:

Client Name Los Angeles County Sanitation Districts

Address 1955 Workman Mill Rd., Whittier Ca 90601

Contact Name Michael Tatlovich

Contact Email mtatlovich@lacsds.org

Contact Phone Number 1.562.699.7411

Project Description Engineering support for construction of a 10-foot-diameter 1.5-mile long tunnel for 8-foot-diameter RCP sewer.

Project Start Date October 2008

Project End Date January 2015

Project Amount: \$150,000.00+ (Est)

Client Name LADWP

Address 919 South Soto Street, Los Angeles Ca 90023

Contact Name Michael Hanson

Contact Email michael.hanson@ladwp.com

Contact Phone Number 1.213.367.1295

Project Description Hydropunching to sample groundwater at LADWP's Coronado Trunk Line Project.

Project Start Date July 2021

Project End Date September 2021

Project Amount: \$26,000.00

Client Name Los Angeles County Sanitation Districts

Address 1955 Workman Mill Rd., Whittier Ca 90601

Contact Name Philip Kang

Contact Email pkang@lacsds.org

Contact Phone Number 1.562.699.7411

Project Description Sewer rehabilitation Projects, drilling borings, performance of field and
and laboratory testing, and preparation of geotechnical data.

Project Start Date October 2020 Project End Date January 2021

Project Amount: Various Projects \$15,000.00 to \$30,000.00

Bidder's Signature  _____

NOTE: If requested by the City, the bidder shall furnish a certified financial statement, references, and other information sufficiently comprehensive to permit an appraisal of his current financial condition.

CLIENT REFERENCE LIST

In order to more fully evaluate your background and experience for the project herein proposed, it is requested that you submit a list of references and/or similar projects completed or in progress within the last 24 months or as noted in the requirements of the RFP or IFB. Your cooperation in this matter is greatly appreciated.

Company Name: Vandermost Consulting Services, Inc. dba VCS Environmental

Number of years as a contractor in the work of this type: _____

Three projects/clients references for this type of work:

Client Name Orange County Transportation Authority

Address 550 S. Main Street, Orange, CA 92868

Contact Name Jason Lee

Contact Email jlee1@octa.net

Contact Phone Number 714-560-5833

Project Description OCTA, in coordination with Metrolink and the cities of Laguna Niguel and San Juan Capistrano, added 1.8 miles of new passing/siding railroad track. The passing/siding track runs adjacent to the existing track, connecting to it at each end.

Project Start Date June 2018

Project End Date Ongoing

Project Amount: \$282,399

Client Name Inland Valley Development Agency

Address 1601 East Third Street, Suite 100, San Bernardino, CA 92408

Contact Name Jim Harris

Contact Email jharris@sbdairport.com

Contact Phone Number 909-382-4100

Project Description The Inland Valley Development Agency is proposing to replace the existing 50-foot-wide Mission Channel bridge on Mission View Avenue with a 98-foot-wide bridge structure.

Project Start Date June 2019

Project End Date September 2020

Project Amount: \$109,204

Client Name Aztec Engineering

Address 2151 Michelson Dr., #100, Irvine, CA 92612

Contact Name Mike Shirley

Contact Email MShirley@aztec.us

Contact Phone Number 602-454-0402

Project Description Tolled express lanes in each direction on Interstate 15 (1-19) in Riverside County from south of
Cajalco Road to State Road 60 (SR-60)

Project Start Date June 2017

Project End Date June 2019

Project Amount: \$394,661

Bidder's Signature Julie Beeman

NOTE: If requested by the City, the bidder shall furnish a certified financial statement, references, and other information sufficiently comprehensive to permit an appraisal of his current financial condition.


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MODIFICATION, CHANGES OR EXCEPTIONS TO THE CITY CONTRACT OF SERVICE AGREEMENT TEMPLATE

MODIFICATIONS TO THE CITY CONTRACT OF SERVICE

Provision	Proposed Amendment	Rationale/Benefit
Scope of Services	In compliance with all terms and conditions of this Agreement, the Consultant shall provide those services specified in the “Scope of Services” attached hereto as Exhibit “A” and incorporated herein by this reference, which may be referred to herein as the “services” or “work” hereunder. As a material inducement to the City entering into this Agreement, Consultant represents and warrants that it has the qualifications, experience, and facilities necessary to properly perform the services required under this Agreement in a thorough, competent, and professional manner, and is experienced in performing the work and services contemplated herein. Consultant shall at all times faithfully, competently and to the best of its ability, experience and talent, perform all services described herein. Consultant covenants that it shall follow the highest professional standards in performing the work and services required hereunder and that all materials will be both of good quality as well as fit for the purpose intended. For purposes of this Agreement, the phrase “highest professional standards” shall mean those standards of practice recognized by one or more first-class firms performing similar work under similar circumstances. Consultant shall perform its services in a manner consistent with the standard of care and skill ordinarily exercised by members of Consultant’s profession practicing in the same or similar locality under the same or similar circumstances at the time the services are performed.	EXP requests the following change as any duty beyond the customary standard of care for professionals is not covered by professional liability policies. Agreeing to any warranty language beyond the customary standard of care could result denial of coverage.
Insurance and indemnification	Professional Liability. Professional liability insurance appropriate to the Consultant’s profession. This coverage may be written on a “claims made” basis, and must include coverage for contractual liability. The professional liability insurance required by this Agreement must be endorsed to be applicable to claims based upon, arising out of or related to services performed under this Agreement.	This language is not insurable for Professional liability. Contractual Liability can be provided under general liability policy but is not commercially available under professional liability policies.
Insurance and indemnification	To the full extent permitted by law, Consultant agrees to indemnify, defend and hold harmless the City, its officers, employees and agents (“Indemnified Parties”) against, and will hold and save them and each of them harmless from, any and all actions, either judicial, administrative, arbitration or regulatory claims, damages to persons or property, losses, costs, penalties, obligations, errors, omissions or liabilities whether actual or threatened (herein “claims or liabilities”) that may be asserted or claimed by any person, firm or entity arising out of or in connection with the <u>to the proportionate extent caused by Consultant’s</u> negligent performance of the work, operations or activities provided herein of Consultant, its officers, employees, agents, subcontractors, or invitees, or any individual or entity for which Consultant is legally liable (“indemnitors”), or arising from <u>to the extent caused by</u> Consultant’s or indemnitors’ reckless or willful misconduct, or arising from Consultant’s or indemnitors’ negligent performance of or failure to perform any term, provision, covenant or condition of this Agreement, and in connection therewith:	For professional services, California Civil Code Section 2782.8 prohibits indemnification except to the extent the claims against the indemnitee ais caused by the negligence, recklessness, or willful misconduct of the design professional. It also prohibits defense costs charged to the design professional from exceeding the design professional's proportionate percentage of fault. We request the following language to clarify that indemnity and defense obligations of Contractor will not extend beyond its proportionate share of fault.
Insurance and indemnification	Consultant will defend any action or actions filed in connection with any of said claims or liabilities and will pay all costs and expenses, including legal costs and attorneys’ fees incurred in connection therewith Subject to California Civil Code Section 2782.8, The Consultant shall not have an upfront duty to defend the City but shall reimburse reasonable defense fees and costs to the extent a Claim is determined to have been caused by the negligence, recklessness or willful misconduct of Consultant, or as the parties otherwise agree in settlement. In no event shall the cost to defend charged to the Consultant exceed the Consultant’s proportionate percentage of fault; Consultant will promptly pay any judgment rendered against the City, its officers, agents or employees for any such claims or liabilities arising out of or in connection with <u>to the proportionate extent caused by</u> the negligent performance of or failure to perform such work, operations or activities of Consultant hereunder; and Consultant agrees to save and hold the City, its officers, agents, and employees harmless therefrom; In the event the City, its officers, agents or employees is made a party to any action or proceeding filed or prosecuted against Consultant for such damages or other claims arising out of or in connection with the negligent performance of or failure to perform the work, operation or activities of Consultant hereunder, Consultant agrees, <u>upon final determination of allowable damages</u> , to pay to the City, its officers, agents or employees, any and all costs and expenses incurred by the City, its officers, agents or employees in such action or proceeding, including but not limited to, <u>reasonable</u> legal costs and attorneys’ fees. If Consultant’s obligation to indemnify, and/or hold harmless arises out of Consultant’s performance as a “design professional” (as that term is defined under Civil Code section 2782.8), then, Consultant’s obligation to indemnify hereunder shall not exceed Consultant’s proportionate share of negligence in its performance of services under this Agreement.	We request the following language to clarify that Indemnity obligations would be subject to California Civil Code Section 2782.8.
Insurance and indemnification	Add to the end of the Paragraph: <u>To the fullest extent permitted by law, in no event will either party be liable or responsible to the other for any type of incidental, indirect or consequential damages arising out of or in connection with this Agreement.</u>	Industry standard design contracts provide a mutual waiver of consequential damages. EXP request that this language be added to clarify allocation of risk for both parties



A photograph of a modern building with palm trees and a fountain, overlaid with a large blue circular graphic. The building has a sign that reads "PACIFIC CITY HALL". The fountain is in the foreground, and the palm trees are in the background. The text "AFFIDAVIT OF NON-COLLUSION" is overlaid on the image.

AFFIDAVIT OF NON-COLLUSION

CITY OF CARSON
AFFIDAVIT OF NON-COLLUSION AND NON-DISCRIMINATION

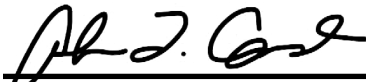
I hereby swear (or affirm) under the penalty of perjury:

That the attached proposal or bid has been arrived at by the responder independently and has been submitted without collusion with and without any agreement, understanding, or planned common course of action with any other firm or entity designed to limit fair and open competition;

That the contents of the proposal or bid response have not been communicated by the responder or its employees or agents to any person not an employee or agent of the responder and will not be communicated to any such persons prior to the official opening of the solicitation responses; and

The proposer/bidder does not and shall not discriminate, will provide equal employment practices, and will adhere to an affirmative action program to ensure that in their employment practices, persons are employed and employees are treated equally and without regard to or because of race, religion, ancestry, national origin, sex, sexual orientation, age, disability, marital status or medical condition.

I certify that the statements in this affidavit are true and accurate.



Signature

 Anh Case, PE, CCM

Printed Name

December 2, 2021

Date

 VP, Director of Construction
 Management - Southern California

Title

CITY OF CARSON
AFFIDAVIT OF NON-COLLUSION AND NON-DISCRIMINATION

I hereby swear (or affirm) under the penalty of perjury:

That the attached proposal or bid has been arrived at by the responder independently and has been submitted without collusion with and without any agreement, understanding, or planned common course of action with any other firm or entity designed to limit fair and open competition;

That the contents of the proposal or bid response have not been communicated by the responder or its employees or agents to any person not an employee or agent of the responder and will not be communicated to any such persons prior to the official opening of the solicitation responses; and

The proposer/bidder does not and shall not discriminate, will provide equal employment practices, and will adhere to an affirmative action program to ensure that in their employment practices, persons are employed and employees are treated equally and without regard to or because of race, religion, ancestry, national origin, sex, sexual orientation, age, disability, marital status or medical condition.

I certify that the statements in this affidavit are true and accurate.



Signature

November 18, 2021

Date

Arvin K. Chaudhary, PE, PLS

Printed Name

President

Title

CITY OF CARSON
AFFIDAVIT OF NON-COLLUSION AND NON-DISCRIMINATION


I hereby swear (or affirm) under the penalty of perjury:

That the attached proposal or bid has been arrived at by the responder independently and has been submitted without collusion with and without any agreement, understanding, or planned common course of action with any other firm or entity designed to limit fair and open competition;

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I certify that the statements in this affidavit are true and accurate.



Signature

November 12th, 2021

Date

Edward Salcedo, Jr.

Printed Name

President

Title

CITY OF CARSON
AFFIDAVIT OF NON-COLLUSION AND NON-DISCRIMINATION

I hereby swear (or affirm) under the penalty of perjury:

That the attached proposal or bid has been arrived at by the responder independently and has been submitted without collusion with and without any agreement, understanding, or planned common course of action with any other firm or entity designed to limit fair and open competition;

That the contents of the proposal or bid response have not been communicated by the responder or its employees or agents to any person not an employee or agent of the responder and will not be communicated to any such persons prior to the official opening of the solicitation responses; and

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I certify that the statements in this affidavit are true and accurate.



11/18/2021

Signature

Date

David Hamilton

Principal

Printed Name

Title

CITY OF CARSON
AFFIDAVIT OF NON-COLLUSION AND NON-DISCRIMINATION

I hereby swear (or affirm) under the penalty of perjury:

That the attached proposal or bid has been arrived at by the responder independently and has been submitted without collusion with and without any agreement, understanding, or planned common course of action with any other firm or entity designed to limit fair and open competition;

That the contents of the proposal or bid response have not been communicated by the responder or its employees or agents to any person not an employee or agent of the responder and will not be communicated to any such persons prior to the official opening of the solicitation responses; and

The proposer/bidder does not and shall not discriminate, will provide equal employment practices, and will adhere to an affirmative action program to ensure that in their employment practices, persons are employed and employees are treated equally and without regard to or because of race, religion, ancestry, national origin, sex, sexual orientation, age, disability, marital status or medical condition.

I certify that the statements in this affidavit are true and accurate.

Julie Beeman
Signature

Julie Beeman

Printed Name

11/17/2021
Date

President

Title

A low-angle, upward-looking photograph of the U.S. Capitol dome. The dome is white with a dark, ribbed top and a golden orb at the very peak. Below the dome, the ornate classical architecture is visible, featuring rows of white columns and arched windows. An American flag is partially visible on the right side of the frame. A large, semi-transparent blue circle is overlaid on the right side of the image, containing the number '10'.

10

FEDERAL LOBBYIST REQUIREMENTS

FEDERAL LOBBYIST REQUIREMENTS CERTIFICATION

Name of Firm: EXP U.S. Services Inc. Date: December 2, 2021
 Address: 451 East Vanderbilt Way, San Bernardino CA
 State: CA Zip Code: 92408 Phone No.: 1.714.423.8171

Acting on behalf of the above-named firm, as its Authorized Official, I certify as follows:

1. No Federal appropriated funds have been paid, by or on behalf of the above named firm to any person for influencing or attempting to influence an officer or employee of any agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of and Federal grant, loan or cooperative agreement, and any extension, continuation, renewal, amendment, or modification thereof, and;
2. If any funds other than Federal appropriated funds have paid or will be paid to any person for influencing or attempting to influence an officer or employee or any agency, a Member of Congress an officer or employee of Congress or an employee of a Member of Congress in connection with this Federal contract, grant loan, or cooperative agreement, the above named firm shall complete and submit Standard Form-LLL, "Disclosure Form to Report Lobbying", in accordance with its instructions, and;
3. The above-named firm shall require that the language of this certification be included in the award documents for all sub-awards at all tiers (including subcontracts, sub-grants, and contracts under grants, loans, and cooperative agreement) and that all sub-recipients shall certify and disclose accordingly.

This certification is a material representation of fact upon which reliance was placed when this transaction was made or entered into. Submission of this certification is a prerequisite for making or entering into the transaction imposed by Section 1352 Title 31, U.S. Code. Any person who fails to file the required certification shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.

Authorized Official:

Name: Anh Case, PE, CCM

VP, Director of Construction Management

Title: - Southern California

Signature: 

Date: December 2, 2021

**FEDERAL LOBBYIST
REQUIREMENTS CERTIFICATION**

Name of Firm: Chaudhary & Associates, Inc. Date: November 18, 2021
Address: 211 Gateway Road West, Suite 204, Napa
State: CA Zip Code: 94558 Phone No.: 707-255-2729

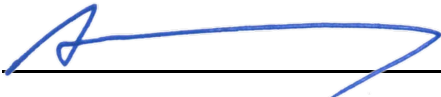
Acting on behalf of the above-named firm, as its Authorized Official, I certify as follows:

1. No Federal appropriated funds have been paid, by or on behalf of the above named firm to any person for influencing or attempting to influence an officer or employee of any agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of and Federal grant, loan or cooperative agreement, and any extension, continuation, renewal, amendment, or modification thereof, and;
2. If any funds other than Federal appropriated funds have paid or will be paid to any person for influencing or attempting to influence an officer or employee or any agency, a Member of Congress an officer or employee of Congress or an employee of a Member of Congress in connection with this Federal contract, grant loan, or cooperative agreement, the above named firm shall complete and submit Standard Form-LLL, "Disclosure Form to Report Lobbying", in accordance with its instructions, and;
3. The above-named firm shall require that the language of this certification be included in the award documents for all sub-awards at all tiers (including subcontracts, sub-grants, and contracts under grants, loans, and cooperative agreement) and that all sub-recipients shall certify and disclose accordingly.

This certification is a material representation of fact upon which reliance was placed when this transaction was made or entered into. Submission of this certification is a prerequisite for making or entering into the transaction imposed by Section 1352 Title 31, U.S. Code. Any person who fails to file the required certification shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.

Authorized Official:

Name: Arvin K. Chaudhary, PE, PLS Title: President

Signature:  Date: November 18, 2021

**FEDERAL LOBBYIST
REQUIREMENTS CERTIFICATION**

Name of Firm: GCAP Services, Inc. Date: November 12th, 2021

Address: 3525 Hyland Ave, Suite 140

State: CA Zip Code: 92626 Phone No.: (714) 800-1795

Acting on behalf of the above-named firm, as its Authorized Official, I certify as follows:

1. No Federal appropriated funds have been paid, by or on behalf of the above named firm to any person for influencing or attempting to influence an officer or employee of any agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of and Federal grant, loan or cooperative agreement, and any extension, continuation, renewal, amendment, or modification thereof, and;

2. If any funds other than Federal appropriated funds have paid or will be paid to any person for influencing or attempting to influence an officer or employee or any agency, a Member of Congress an officer or employee of Congress or an employee of a Member of Congress in connection with this Federal contract, grant loan, or cooperative agreement, the above named firm shall complete and submit Standard Form-LLL, "Disclosure Form to Report Lobbying", in accordance with its instructions, and:

3. The above-named firm shall require that the language of this certification be included in the award documents for all sub-awards at all tiers (including subcontracts, sub-grants, and contracts under grants, loans, and cooperative agreement) and that all sub-recipients shall certify and disclose accordingly.

This certification is a material representation of fact upon which reliance was placed when this transaction was made or entered into. Submission of this certification is a prerequisite for making or entering into the transaction imposed by Section 1352 Title 31, U.S. Code. Any person who fails to file the required certification shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.

Authorized Official:

Name: Edward Salcedo, Jr. Title: Presid

Signature:  Date: November 12th, 2021

**FEDERAL LOBBYIST
REQUIREMENTS CERTIFICATION**


Name of Firm: Hamilton & Associates, Inc. Date: 11/18/2021
Address: 1641 Border Avenue, Torrance
State: Ca Zip Code: 90501 Phone No.: (310) 618-2190

Acting on behalf of the above-named firm, as its Authorized Official, I certify as follows:

1. No Federal appropriated funds have been paid, by or on behalf of the above named firm to any person for influencing or attempting to influence an officer or employee of any agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of and Federal grant, loan or cooperative agreement, and any extension, continuation, renewal, amendment, or modification thereof, and;
2. If any funds other than Federal appropriated funds have paid or will be paid to any person for influencing or attempting to influence an officer or employee or any agency, a Member of Congress an officer or employee of Congress or an employee of a Member of Congress in connection with this Federal contract, grant loan, or cooperative agreement, the above named firm shall complete and submit Standard Form-LLL, "Disclosure Form to Report Lobbying", in accordance with its instructions, and;
3. The above-named firm shall require that the language of this certification be included in the award documents for all sub-awards at all tiers (including subcontracts, sub-grants, and contracts under grants, loans, and cooperative agreement) and that all sub-recipients shall certify and disclose accordingly.

This certification is a material representation of fact upon which reliance was placed when this transaction was made or entered into. Submission of this certification is a prerequisite for making or entering into the transaction imposed by Section 1352 Title 31, U.S. Code. Any person who fails to file the required certification shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.

Authorized Official:

Name: David Hamilton Title: Principal
Signature:  Date: 11/18/2021

**FEDERAL LOBBYIST
REQUIREMENTS CERTIFICATION**

Name of Firm: Vandermost Consulting Services, Inc. dba VCS Environmental Date: 11/17/2021

Address: 30900 Rancho Viejo Road, Suite 100, San Juan Capistrano

State: California Zip Code: 92675 Phone No.: 949-489-2700

Acting on behalf of the above-named firm, as its Authorized Official, I certify as follows:

1. No Federal appropriated funds have been paid, by or on behalf of the above named firm to any person for influencing or attempting to influence an officer or employee of any agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of and Federal grant, loan or cooperative agreement, and any extension, continuation, renewal, amendment, or modification thereof, and;
2. If any funds other than Federal appropriated funds have paid or will be paid to any person for influencing or attempting to influence an officer or employee or any agency, a Member of Congress an officer or employee of Congress or an employee of a Member of Congress in connection with this Federal contract, grant loan, or cooperative agreement, the above named firm shall complete and submit Standard Form-LLL, "Disclosure Form to Report Lobbying", in accordance with its instructions, and;
3. The above-named firm shall require that the language of this certification be included in the award documents for all sub-awards at all tiers (including subcontracts, sub-grants, and contracts under grants, loans, and cooperative agreement) and that all sub-recipients shall certify and disclose accordingly.

This certification is a material representation of fact upon which reliance was placed when this transaction was made or entered into. Submission of this certification is a prerequisite for making or entering into the transaction imposed by Section 1352 Title 31, U.S. Code. Any person who fails to file the required certification shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.

Authorized Official:

Name: Julie Beeman Title: President

Signature:  Date: 11/17/2021

11

DEBARMENT AND SUSPENSION CERTIFICATE

CARSON CITY HALL

DEBARMENT AND SUSPENSION CERTIFICATION

Name of Firm: EXP U.S. Services Inc.

Acting on behalf of the above-named firm ("Consultant"), as its Authorized Official, I, the undersigned, certify as follows:

I am a duly authorized representative of ("Consultant"). Consultant certifies, to the best of its knowledge and belief, that Consultant, including its principals:

Is not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any federal department or agency, and not does not have a proposed debarment pending;

Has not within the three-year period preceding this certification been convicted of or had a civil judgment rendered against it for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (federal, state or local) transaction, contract, or subcontract under a public transaction; for violation of federal or state antitrust statutes; or for commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements or receiving stolen property;

Is not presently indicted for or otherwise criminally or civilly charged by a governmental entity (federal, state, or local) with commission of any of the offenses enumerated in paragraph (2) above; and

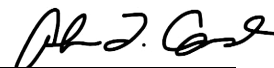
Has not within the three-year period preceding this certification had one or more public transactions (federal, state or local) terminated for cause or default.

Consultant further certifies that Consultant, including its principals, is not listed on the government-wide exclusions in the System for Award Management.

Consultant acknowledges that falsely providing this certification may result in criminal prosecution or administrative sanctions, and that this certification is a required component of all proposals in response to this RFP/IFB.

A proposal that does not include a completed and signed version of this certification will be deemed incomplete and materially nonresponsive, and will not be considered.

CONSULTANT

By: Anh Case, PE, CCM 
 Title: VP, Director of Construction
 Management - Southern California

Date: December 2, 2021

DEBARMENT AND SUSPENSION CERTIFICATION

Name of Firm: Chaudhary & Associates, Inc.

Acting on behalf of the above-named firm ("Consultant"), as its Authorized Official, I, the undersigned, certify as follows:

I am a duly authorized representative of ("Consultant"). Consultant certifies, to the best of its knowledge and belief, that Consultant, including its principals:

Is not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any federal department or agency, and not does not have a proposed debarment pending;

Has not within the three-year period preceding this certification been convicted of or had a civil judgment rendered against it for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (federal, state or local) transaction, contract, or subcontract under a public transaction; for violation of federal or state antitrust statutes; or for commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements or receiving stolen property;

Is not presently indicted for or otherwise criminally or civilly charged by a governmental entity (federal, state, or local) with commission of any of the offenses enumerated in paragraph (2) above; and

Has not within the three-year period preceding this certification had one or more public transactions (federal, state or local) terminated for cause or default.

Consultant further certifies that Consultant, including its principals, is not listed on the government-wide exclusions in the System for Award Management.

Consultant acknowledges that falsely providing this certification may result in criminal prosecution or administrative sanctions, and that this certification is a required component of all proposals in response to this RFP/IFB.

A proposal that does not include a completed and signed version of this certification will be deemed incomplete and materially nonresponsive, and will not be considered.

CONSULTANT

By:  _____

Title: Arvin K. Chaudhary, PE, PLS, President

Date: November 18, 2021

DEBARMENT AND SUSPENSION CERTIFICATION

Name of Firm: GCAP Services, Inc.

Acting on behalf of the above-named firm ("Consultant"), as its Authorized Official, I, the undersigned, certify as follows:

I am a duly authorized representative of ("Consultant"). Consultant certifies, to the best of its knowledge and belief, that Consultant, including its principals:

Is not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any federal department or agency, and not does not have a proposed debarment pending;

Has not within the three-year period preceding this certification been convicted of or had a civil judgment rendered against it for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (federal, state or local) transaction, contract, or subcontract under a public transaction; for violation of federal or state antitrust statutes; or for commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements or receiving stolen property;

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A proposal that does not include a completed and signed version of this certification will be deemed incomplete and materially nonresponsive, and will not be considered.

CONSULTANT

By: 
Edward Salcedo, Jr.

Title: President

Date: November 12th, 202

DEBARMENT AND SUSPENSION CERTIFICATION

Name of Firm: Hamilton & Associates, Inc.

Acting on behalf of the above-named firm ("Consultant"), as its Authorized Official, I, the undersigned, certify as follows:

I am a duly authorized representative of ("Consultant"). Consultant certifies, to the best of its knowledge and belief, that Consultant, including its principals:

Is not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any federal department or agency, and not does not have a proposed debarment pending;

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Consultant acknowledges that falsely providing this certification may result in criminal prosecution or administrative sanctions, and that this certification is a required component of all proposals in response to this RFP/IFB.

A proposal that does not include a completed and signed version of this certification will be deemed incomplete and materially nonresponsive, and will not be considered.

CONSULTANT

By: 

Principal

Title: _____

Date: _____

11/18/2021

DEBARMENT AND SUSPENSION CERTIFICATION

Name of Firm: Vandermost Consulting Services, Inc dba VCS Environmental

Acting on behalf of the above-named firm ("Consultant"), as its Authorized Official, I, the undersigned, certify as follows:

I am a duly authorized representative of ("Consultant"). Consultant certifies, to the best of its knowledge and belief, that Consultant, including its principals:

Is not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any federal department or agency, and not does not have a proposed debarment pending;

Has not within the three-year period preceding this certification been convicted of or had a civil judgment rendered against it for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (federal, state or local) transaction, contract, or subcontract under a public transaction; for violation of federal or state antitrust statutes; or for commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements or receiving stolen property;

Is not presently indicted for or otherwise criminally or civilly charged by a governmental entity (federal, state, or local) with commission of any of the offenses enumerated in paragraph (2) above; and

Has not within the three-year period preceding this certification had one or more public transactions (federal, state or local) terminated for cause or default.

Consultant further certifies that Consultant, including its principals, is not listed on the government-wide exclusions in the System for Award Management.

Consultant acknowledges that falsely providing this certification may result in criminal prosecution or administrative sanctions, and that this certification is a required component of all proposals in response to this RFP/IFB.

A proposal that does not include a completed and signed version of this certification will be deemed incomplete and materially nonresponsive, and will not be considered.

CONSULTANT

By: Julie Bremer

Title: President

Date: 11/17/2021

12

CERTIFICATE OF COMPLIANCE WITH LABOR CODE 3700

CITY OF CARSON
CITY OF CARSON

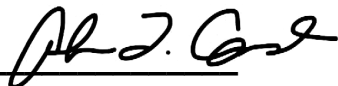
**CERTIFICATE OF COMPLIANCE WITH LABOR CODE SECTION
3700**

Name of Firm: EXP U.S. Services Inc.

Acting on behalf of the above-named firm ("Consultant"), as its Authorized Official, I, the undersigned, certify as follows:

Consultant is aware of the provisions of Section 3700 of the California Labor Code, which require every employer to be insured against liability for workers' compensation or to undertake self-insurance in accordance with provisions of that code, and will comply with such provisions before commencing the performance of the work under any contract awarded in response to Consultant's proposal.

CONSULTANT

By: Anh Case, PE, CCM 
VP, Director of Construction
Title: Management - Southern California
Date: December 2, 2021

**CERTIFICATE OF COMPLIANCE WITH LABOR CODE SECTION
3700**

Name of Firm: Chaudhary & Associates, Inc.

Acting on behalf of the above-named firm ("Consultant"), as its Authorized Official, I, the undersigned, certify as follows:

Consultant is aware of the provisions of Section 3700 of the California Labor Code, which require every employer to be insured against liability for workers' compensation or to undertake self-insurance in accordance with provisions of that code, and will comply with such provisions before commencing the performance of the work under any contract awarded in response to Consultant's proposal.

CONSULTANT

By:  _____

Title: Arvin K. Chaudhary, PE, PLS, President
Date: November 18, 2021

**CERTIFICATE OF COMPLIANCE WITH LABOR CODE SECTION
3700**

Name of Firm: Hamilton & Associates, Inc

Acting on behalf of the above-named firm ("Consultant"), as its Authorized Official, I, the undersigned, certify as follows:

Consultant is aware of the provisions of Section 3700 of the California Labor Code, which require every employer to be insured against liability for workers' compensation or to undertake self-insurance in accordance with provisions of that code, and will comply with such provisions before commencing the performance of the work under any contract awarded in response to Consultant's proposal.

CONSULTANT

By: 

Title: Principal

Date: 11/18/2021

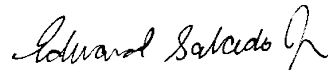
**CERTIFICATE OF COMPLIANCE WITH LABOR CODE SECTION
3700**

Name of Firm: GCAP Services, Inc.

Acting on behalf of the above-named firm ("Consultant"), as its Authorized Official, I, the undersigned, certify as follows:

Consultant is aware of the provisions of Section 3700 of the California Labor Code, which require every employer to be insured against liability for workers' compensation or to undertake self-insurance in accordance with provisions of that code, and will comply with such provisions before commencing the performance of the work under any contract awarded in response to Consultant's proposal.

CONSULTANT



By: Edward Salcedo, Jr.

Title: President

Date: November 12th, 2021

**CERTIFICATE OF COMPLIANCE WITH LABOR CODE SECTION
3700**

Name of Firm: Vandermost Consulting Services, Inc. dba VCS Environmental

Acting on behalf of the above-named firm ("Consultant"), as its Authorized Official, I, the undersigned, certify as follows:

Consultant is aware of the provisions of Section 3700 of the California Labor Code, which require every employer to be insured against liability for workers' compensation or to undertake self-insurance in accordance with provisions of that code, and will comply with such provisions before commencing the performance of the work under any contract awarded in response to Consultant's proposal.

CONSULTANT

By: Julie Bremer

Title: President

Date: 11/17/2021



APPENDIX: ADDENDUM NO.1

ADDENDUM NO. 1

RFP 21-040

CONSTRUCTION MANAGEMENT FOR SEPULVEDA BLVD. WIDENING PROJECT

October 26, 2021

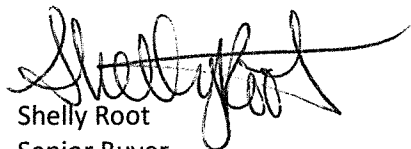
TO ALL BIDDERS:

The following addendum provides the following information to be incorporated into the bid document per this Addendum.

Pre-Proposal Meeting is Optional and scheduled for 10/28/2021 | 10:00AM
Location: Sepulveda Bridge over Dominguez Channel

Please sign below and attach this "Acknowledgment of Receipt" of Addendum 1 proposal to submittal documents. Failure to acknowledge this Addendum may result in your submittal being deemed non-responsive.

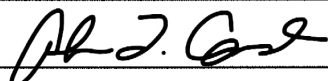
Sincerely,



Shelly Root
Senior Buyer

October 26, 2021

ADDENDUM ACKNOWLEDGEMENT:

Proposer Firm Name: EXP U.S. Services Inc.Authorized Signature:  Date: December 2, 2021



EXP

451 East Vanderbilt Way | San Bernardino, CA 92408

t: 1.909.751.3250

exp.com